Meeting Packet
June 3, 2013, 5:00 p.m.

Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

North Central Florida Regional Planning Council

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2009 NW 67th Place, Gainesville, FL 32653-1603 • 352.955.2200

May 27, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Randy Wells, Chair

SUBJECT: Meeting Announcement

The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area will meet on **Monday, June 3, 2013 at 5:00 p.m.** This meeting will be held in the **Jack Durrance Auditorium, Alachua County Administration Building,** Gainesville, Florida.

Attached are copies of the meeting agenda.

If you have any questions concerning this matter, please contact Marlie Sanderson, AICP, Director of Transportation Planning, at 352.955.2200, extension 103.

Attachments

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AGENDA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA

Jack Durrance Auditorium
Alachua County Administration Building, Gainesville, Florida

Monday, 5:00 p.m.
June 3, 2013

STAFF RECOMMENDATION

Page #3 **I. Approval of the Meeting Agenda and Consent Agenda Items** — **APPROVE BOTH AGENDAS**

The MTPO needs to approve the meeting agenda and the consent agenda items.

Page #157 **II. Citizen Advisory Committee Vacant Position** — **FILL ONE VACANT POSITION**

The MTPO needs to fill one vacant position on this Committee.

Page #177 **III. Certification of the Metropolitan Transportation Planning Process** — **APPROVE CERTIFICATION**

Approval of this certification is on the regular agenda so that the public can be given an opportunity to speak about the recommendation to certify the MTPO Program this year.

Page #183 **IV. Transportation Improvement Program Amendments for Fiscal Years 2012-13 to 2016-17** — **APPROVE AMENDMENTS**

Three amendments are needed because right-of-way activity is being added to Fiscal Year 2013-14 for portions of the Archer Braid Bicycle/Pedestrian Trail.

Page #187 **V. Transportation Improvement Program for Fiscal Years 2013-14 to 2017-18** — **APPROVE JOINT RECOMMENDATION**

All federal funds in this Program have to be approved by the MTPO.

Page #195 **VI. List of Priority Projects** **APPROVE JOINT RECOMMENDATION**

This time each year, the MTPO approves lists of projects that are needed, but not funded.

Page #205 **VII. Regional Transit System Alternatives Analysis Study** **NO ACTION REQUIRED**

Regional Transit System staff has requested the opportunity to provide updated information concerning this Study.

Page #209 **VIII. Marking Pedestrian Crossings** **NO ACTION REQUIRED**

A member of the MTPO has requested the opportunity to discuss this issue.

Page #211 **IX. Four-laning of Archer Road- Tower Road to the City of Archer** **NO ACTION REQUIRED**

The City of Archer and a member of the MTPO wants to know "what would it take and cost to four-lane Archer Road to the City of Archer?"

Page #219 **X. Available Federal Funds for Tower Road** **NO ACTION REQUIRED**

A member of the MTPO wants to know "what federal funds are available for the Tower Road Project."

Back Cover **XI. Next MTPO Meeting** **NO ACTION REQUIRED**

The next MTPO meeting is scheduled for August 5, 2013 at 3:00 p.m.

XII. Comments

A. MTPO Members*
B. Citizens Comments*
C. Chair's Report*

If you have any questions concerning this matter, please contact Marlie Sanderson, AICP, Director of Transportation Planning, at 352.955.2200.

*No backup material included with the attached agenda material.

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CONSENT AGENDA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA

Jack Durrance Auditorium
Alachua County Administration Building, Gainesville Florida

Monday, 5:00 p.m.
June 3, 2013

STAFF RECOMMENDATION

Page #7 **CA. 1** **Minutes- February 4, 2013** **APPROVE MINUTES**

This set of MTPO minutes is ready for review.

Page #17 **CA. 2** **Minutes- March 11, 2013** **APPROVE MINUTES**

This set of MTPO minutes is ready for review.

Page #25 **CA. 3** **Nondiscrimination Policy Statement** **APPROVE STAFF RECOMMENDATION**

Each year, this form needs to be executed and submitted to the Florida Department of Transportation.

Page #29 **CA. 4** **Auditor Selection Process** **APPROVE BYLAWS**

The Regional Planning Council and the MTPO jointly select an auditor to prepare a joint audit.

Page #31 **CA. 5** **Archer Road at SW 34th Street Intersection Modifications** **APPROVE STAFF RECOMMENDATION**

The Florida Department of Transportation has prepared a Traffic Operations Study to improve traffic flow for motorists at this intersection.

Page #61 **CA. 6** **SW 8th Avenue Multi-Use Path- 60 Percent Design Plans** **APPROVE STAFF RECOMMENDATION**

In October 2012, the MTPO approved "scoping plans" for this project and requested that Alachua County staff bring back 60 percent plans for the MTPO to review.

Page #115 **CA. 7** **Transportation Disadvantaged Program- Coordinating Board Appointment** **APPOINT DR. LEVY**

Dr. Levy is the only applicant for the voting Elderly Representative position on this Board.

Page #119 **CA. 8** **Transportation Disadvantaged Program- Planning Grant Resolution, Fiscal Year 2013-14** **APPROVE RESOLUTION**

This resolution and planning agreement are for funds that are used to provide staff services to the Transportation Disadvantaged Coordinating Board.

Page #137 **CA. 9** **Transportation Disadvantaged Program- Status Report** **NO ACTION REQUIRED**

The MTPO has asked for regular status reports concerning this program.

Page #149 **CA. 10** **City Beautification Board Letter** **NO ACTION REQUIRED**

The enclosed letter to Mayor Lowe states that the painting of mast arms is an important design feature that needs to be addressed and funded.

Page #151 **CA. 11** **SR 329 (Main Street) Transfer from Depot Avenue to SR 331 (Williston Road)** **NO ACTION REQUIRED**

The enclosed FDOT letter encourages the City and County to work together to reach an agreement on which agency should assume ownership and maintenance of Main Street.

Page #153 **CA. 12** **Funding Florida's Future Transportation System** **NO ACTION REQUIRED**

Enclosed is the MPOAC Transportation Revenue Study.

Consent

Agenda

Enclosures

MINUTES
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA

Jack Durrance Auditorium
Alachua County Administration Building
Gainesville, Florida

3:00 p.m.
Monday
February 4, 2013

MEMBERS PRESENT
Susan Baird, Vice Chair
Susan Bottcher
Mike Byerly
Chuck Chestnut
Gib Coerper
James Bennett/Greg Evans
Thomas Hawkins
Yvonne Hinson-Rawls
Robert Hutchinson
Craig Lowe
Lee Pinkoson
Lauren Poe
Curtis Reynolds

MEMBERS ABSENT
Randy Wells, Chair
Todd Chase

OTHERS PRESENT
See Exhibit A

STAFF PRESENT
Scott Koons
Marlie Sanderson
Michael Escalante

CALL TO ORDER

Commissioner Susan Baird chaired the meeting because Chair Randy Wells was absent. She called the meeting to order at 3:05 p.m.

I. APPROVAL OF THE MEETING AGENDA AND CONSENT AGENDA

Mr. Marlie Sanderson, Director of Transportation Planning, recommended approval of the consent agenda and meeting agenda.

Several members wanted to know if there were any citizens present who wanted to discuss the Archer Braid Trail. Chair Baird asked if there were any citizens interested in providing comments on the Archer Braid Trail. No one came forward to indicate that they wanted to speak.

MOTION: Commissioner Pinkoson moved to approve the Consent Agenda and Meeting Agenda. Commissioner Hawkins seconded; motion passed unanimously.

II. DR. KERMIT SIGMON CITIZEN PARTICIPATION AWARD

Mr. Sanderson stated that University of Florida Director of Transportation and Parking Services Scott Fox was selected to receive the 2010 Dr. Kermit Sigmon Citizen Participation Award. He discussed Mr. Fox's service to the MTPO and the community. He presented him the award.

Mr. Fox discussed his service and thanked the MTPO.

Chair Baird thanked Mr. Fox for his service to the community.

III. ALACHUA COUNTY INTERSTATE 75 INTELLIGENT TRANSPORTATION SYSTEM (ITS) PROJECT UPDATE

Mr. Sanderson stated that Florida Department of Transportation staff has an opportunity to update the MTPO on the status of ITS on I-75.

Mr. Peter Vega, Florida Department of Transportation District 2 Intelligent Transportation System Engineer, discussed the status of the Interstate 75 Intelligent Transportation System Project and answered questions.

IV. PLANNING AREA BOUNDARY, VOTING MEMBERS AND VOTING PROCEDURES

Mr. Sanderson stated that the MTPO, at its December meeting, authorized staff to prepare a report over the next six months concerning the advantages and disadvantages of expanding the metropolitan planning area boundary to include all of Alachua County, including corresponding changes that would be needed to existing membership and voting procedures based on the 2010 census. He discussed draft alternative planning area boundary maps, voting membership and voting procedures and answered questions. He noted that staff has visited some outlying municipalities and met twice with the Alachua County League of Cities.

Mr. James Bennett, Florida Department of Transportation District 2 Urban Transportation Development Engineer, discussed the minimum metropolitan planning area boundary requirements and funding issues.

City of Hawthorne Mayor Mathew Surrency discussed the planning area boundary, membership and voting procedures.

MOTION: Commissioner Hutchinson moved to approve the Option 2 Map with the addition of voting membership to the City of Alachua and a representative from the Alachua County League of Cities and simple majority voting. Commissioner Pinkoson seconded. After discussion, Commissioner Bottcher moved to split the motion.

SUBSTITUTE MOTION:

Commissioner Pinkoson moved to keep Option 1. Mayor Lowe seconded.

MODIFIED SUBSTITUTE MOTION:

Commissioner Pinkoson moved to keep the Option 1 Map and address all federal requirements. Mayor Lowe seconded.

SECOND SUBSTITUTE MOTION

Commissioner Hinson-Rawls moved to approve the Option 2 Map and keep the existing voting membership and voting procedures. Commissioner Hutchinson seconded. After discussion, Commissioner Byerly move to split the motion.

SPLIT SECOND SUBSTITUTE MOTION- PART ONE

Commissioner Hinson-Rawls moved to approve the Option 2 Map. Commissioner Hutchinson seconded; motion passed 8 to 2 with Commissioner Pinkoson and Chair Baird in dissent.

SPLIT SECOND SUBSTITUTE MOTION- PART TWO

Commissioner Hinson-Rawls moved to keep the existing voting membership and voting procedures. Commissioner Hutchinson seconded; motion failed for lack of a City Commission majority.

MOTION: Commissioner Poe moved for staff to come back at the June meeting with additional voting configurations and transition plans. Commissioner Bottcher seconded.

A member of the MTPO stated that the outlying municipalities do not want the MTPO to expand its metropolitan planning area boundary to include all of Alachua County, and therefore, the MTPO should just keep its current voting membership and voting procedures along with earlier decision to approve the Option 2 map.

MODIFIED MOTION:

Commissioner Poe moved for staff to come back at the June meeting with voting configurations that exclude the outlying municipalities and transition plans. Commissioner Bottcher seconded; motion failed because a majority of the County Commission voted against the motion.

Chair Baird stated that, since the MTPO approved the Option 2 metropolitan planning area map earlier in the meeting, the final outcome is that the MTPO will be keeping its existing MTPO voting membership and voting procedures along with the metropolitan planning area boundary in Option 2.

V. HULL ROAD EXTENSION- RIGHT-OF-WAY WIDTH

Mr. Gerry Dedenbach, Causseaux, Hewett & Wapole Director of Planning & GIS Services, gave a presentation concerning the Village Point project and answered questions.

MOTION: Commissioner Pinkoson moved to reduce the Hull Road Extension right-of-way width from 100 feet to 90 feet within the Village Point Project. Mayor Lowe Seconded; motion passed unanimously.

VI. LISTENING TOUR

Mr. Sanderson discussed the status of the "Listening Tour" and answered questions.

Mr. Mark Sexton, Alachua County Communications Director, discussed the proposed Alachua County Summit that will be held on March 27, 2013 and April 10, 2013.

Chair Baird noted that the March 27th date coincided with the Alachua County Schools spring break and would not be a good date for the Alachua County Summit.

A member asked staff to contact the City of Gainesville staff to place the "Listening Tour" dates on the calendars for the Gainesville City Commission and to make sure that these meetings are properly noticed so that they are in the "sunshine."
Chair Baird discussed the possibility of using Gainesville Regional Utilities billing as a means to survey the community about transportation issues.

A member requested that a copy of the survey from the Year 2025 Livable Community reinvestment Plan be provided to MTPO members in the next meeting packet.

A member noted that "Open Alachua" is also conducting a transportation survey and that he would look into getting the data when it is available. He also added that he would raise the GRU survey topic at the next City Commission meeting.

A member recommended that discussion items for the "Listening Tour" could be determined later after the Alachua County Summit.

MOTION: Commissioner Hawkins moved to incorporate the "Listening Tour" into the "Joint Alachua County Joint Meetings with Municipalities" schedule and use the August 12, 2013 date for the meeting in Gainesville (see Exhibit 2). Commissioner Byerly seconded; motion passed unanimously.

VII. MULTIMODAL OVERPASS AT SW 34TH STREET AND HULL ROAD

Mr. Sanderson discussed the proposed multimodal overpass at SW 34th Street and Hull Road. He stated that it was his understanding that the Florida Department of Transportation, the City of Gainesville and the University of Florida have all stated that they are not willing to accept maintenance responsibility for this overpass. He also stated that the University of Florida is concerned about the amount of right-of-way that will be needed for such an overpass.

MOTION: Commissioner Hawkins moved to consider including transit in the proposed SW 34th Street and Hull Road overpass during the design phase of this project, or in the long range transportation plan update, whichever comes first. Commissioner Hutchinson seconded; motion passed 7 to 1, with Chair Baird in dissent.

VII. NEXT MEETING

Mr. Sanderson announced that the next meeting is scheduled for March 4th at 3:00 p.m.

VIII. COMMENTS

A. MEMBERS

There were no member comments.

B. CITIZENS

Mr. Richard Miles, Cade Museum Board Vice President, discussed the status of the museum construction and transportation access and answered questions.

C. CHAIR'S REPORT

There was no Chair's Report.

ADJOURNMENT

Chair Baird adjourned the meeting at 5:38 p.m.

_____ _____

Date Lauren Poe, Secretary/Treasurer

EXHIBIT A

Interested Citizens	Alachua County	City of Gainesville	Florida Department of Transportation
Ned Baier	Dave Cerlanek	Russ Blackburn	Karen Taulbee
Gerry Dedenbach	Chris Dawson	Paul Folkers	Peter Vega
Scott Fox	Dave Schwartz	Debbie Leistner	
Beth Lemke	Mark Sexton	Teresa Scott	
Richard Miles	Alan Yeatter		
Wiley Page	Chris Ziegler		
Mathew Surrency			

* By telephone
# Spoke and provided written comments

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EXHIBIT 1

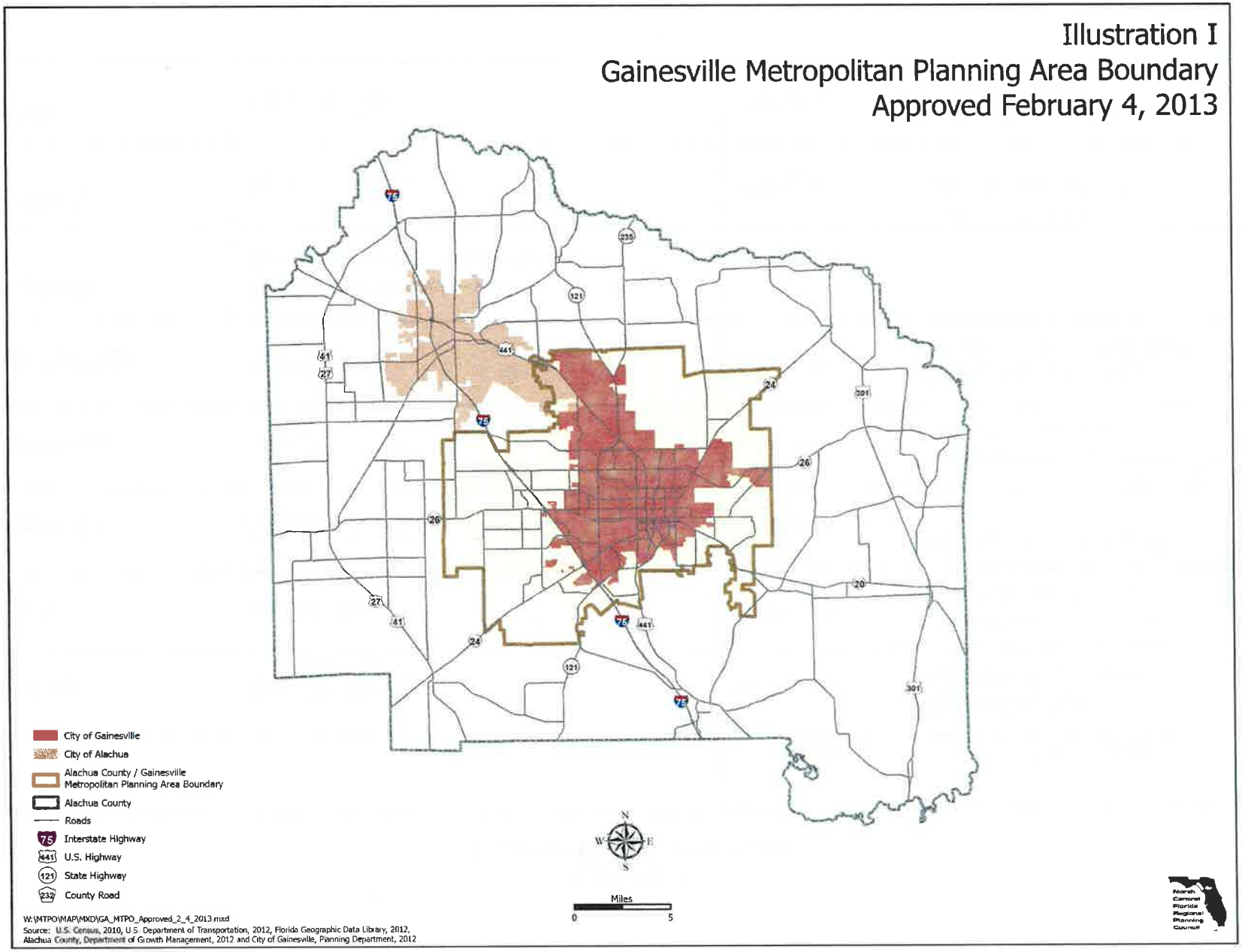
Illustration I
Gainesville Metropolitan Planning Area Boundary
Approved February 4, 2013
City of Gainesville
City of Alachua
Alachua County / Gainesville Metropolitan Planning Area Boundary
Alachua County
Roads
Interstate Highway
U.S. Highway
State Highway
County Road
Miles
0
5
W:\MTPO\MAP\MXD\GA_MTPO_Approved_2_4_2013.mxd
Source: U.S. Census, 2010, U.S. Department of Transportation, 2012, Florida Geographic Data Library, 2012, Alachua County, Department of Growth Management, 2012 and City of Gainesville, Planning Department, 2012
North Central Florida Regional Planning Council

EXHIBIT 2
Tentative "Listening Tour" Dates

City	Date	Time	Location
Alachua	May 20, 2013	6:30 p.m.	Alachua City Hall 15100 NW 142 Terrace
Archer	April 8, 2013	7:00 p.m.	Archer City Hall Chambers 16870 SW 134 Avenue
Gainesville	August 12, 2013	3:00 p.m.	Jack Durrance, Room 209
Hawthorne	May 21, 2013	6:30 p.m.	Hawthorne City Hall Auditorium 6700 S.E. 221 Street
High Springs	May 9, 2013	6:30 p.m.	High Springs City Hall, 110 NW 1st Ave., 2nd Floor
Micanopy	***Waiting to hear back – will know after Micanopy Feb 12th Board Meeting***		
Newberry	April 22, 2013	7:00 p.m.	Newberry City Hall 25440 W Newberry Rd
Waldo	April 18, 2013	7:00 p.m.	Waldo Yerkes Center 14245 Cole Street

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CONSENT AGENDA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA

Jack Durrance Auditorium
Alachua County Administration Building
Gainesville, Florida

Monday, 3:00 p.m.
February 4, 2013

STAFF RECOMMENDATION

Page #7 **CA. 1** **MTPO Minutes- December 3, 2012** **APPROVE MINUTES**

This set of MTPO minutes is ready for review

Page #19 **CA. 2** **Archer Braid Trail- 60 Percent Plans** **APPROVE STAFF RECOMMENDATION**

The Alachua County Public Works Department will present 60 percent plans for this project.

Page #21 **CA. 3** **Fiscal Year 2011-12 Audit** **APPROVE COMMITTEE RECOMMENDATION**

The Audit Review Committee recommends acceptance of the audit report and approval of the invoice for payment.

Page #23 **CA. 4** **Year 2040 Long Range Transportation Plan- Request for Qualifications** **APPROVE REQUEST FOR QUALIFICATIONS**

The Request for Qualifications discusses the process that will be used to select the consultant to assist in preparing the Year 2040 Long Range Transportation Plan.

Page #25 **CA. 5** **Year 2040 Long Range Transportation Plan- Scope of Services** **APPROVE JOINT RECOMMENDATION**

The Scope of Services discusses specific tasks the consultant is responsible for with respect to the Year 2040 Long Range Transportation Plan.

Page #29 **CA. 6** **Continuity of Operations Plan** **APPROVE STAFF RECOMMENDATION**

This Plan is reviewed each year and revisions are made as needed.

Page #31 **CA. 7** **Transportation Disadvantaged Program- Coordinating Board Appointment** **APPROVE STAFF RECOMMENDATION**

The Florida Department of Transportation is recommending that Ms. Janell Damato be appointed as the Department's voting representative.

Page #35 **CA. 8** **Transportation Disadvantaged Program- Coordinating Board Membership Certification** **APPROVE STAFF RECOMMENDATION**

Each year, this certification is approved certifying that the Board represents a cross section of the community.

Page #39 **CA. 9** **Transportation Disadvantaged Program- Status Report** **NO ACTION REQUIRED**

The MTPO has asked for regular status reports concerning this program.

Page #51 **CA. 10** **MPOAC Weekend Institute** **NO ACTION REQUIRED**

Please contact staff if you are interested in attending the MPOAC Weekend Institute.

MINUTES
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA

Jack Durrance Auditorium
Alachua County Administration Building
Gainesville, Florida

5:00 p.m.
Monday
March 11, 2013

MEMBERS PRESENT
Randy Wells, Chair
Susan Baird, Vice Chair
Susan Bottcher
Mike Byerly
Todd Chase
Chuck Chestnut
Yvonne Hinson-Rawls
Robert Hutchinson
Craig Lowe
Lee Pinkoson
Lauren Poe

MEMBERS ABSENT
Gib Coerper
Greg Evans
Thomas Hawkins
Curtis Reynolds

OTHERS PRESENT
See Exhibit A

STAFF PRESENT
Marlie Sanderson
Michael Escalante

CALL TO ORDER

Chair Randy Wells called the meeting to order at 5:05 p.m.

I. APPROVAL OF THE MEETING AGENDA

Mr. Marlie Sanderson, Director of Transportation Planning, recommended approval of the meeting agenda.

MOTION: Commissioner Poe moved to approve the Meeting Agenda. Commissioner Hutchinson seconded; motion passed unanimously.

II. COMMUNITY TRANSPORTATION COORDINATOR (CTC) SELECTION

Mr. Sanderson stated that MV Transportation, Inc. was the only applicant for community transportation coordinator.

Mr. Ed Griffin, MV Transportation Vice President for Business Development/Government Relations, discussed the selection recommendation.

A member asked Mr. Griffin to identify funding sources for providing trips to the Senior Center on NW 34th Street.

A member invited Mr. Griffin to participate in the Alachua County Countywide Transportation Summit that is scheduled for April 10, 2013

Mr. Griffin stated that MV Transportation would attend the Summit.

MOTION: Commissioner Poe moved to recommend that the Florida Commission for the Transportation Disadvantaged designate MV Transportation, Inc. as the Alachua County Community Transportation Coordinator for a five-year period effective July 1, 2013 subject to a favorable recommendation by the Alachua County Transportation Disadvantaged Coordinating Board. Commissioner Bottcher seconded; motion passed unanimously.

III. TRANSPORTATION ALTERNATIVES APPLICATIONS

Mr. Sanderson stated that the Florida Department of Transportation has requested that two Transportation Alternatives Project (TAP) Applications be submitted by March 29, 2013. He discussed the two proposed projects and answered questions.

Mr. Mike Escalante, MTPO Senior Planner, discussed the location of the SW 27th Street/SW 40th Place/SW 25th Terrace sidewalk project.

Ms. Teresa Scott, City of Gainesville Public Works Department Director, discussed the SW 27th Street/SW 40th Place/SW 25th Terrace sidewalk project and noted that the application could specify that the sidewalk will be wide enough to accommodate multi-use activity.

Mr. Jeff Hays, Alachua County Transportation Planning Manager, discussed the Kanapaha Trail project.

Ms. Scott noted that some priorities are identified for funding by sources other than Transportation Alternatives Project funds, and therefore, other priority needs are included on the list.

Mr. Sanderson noted that Florida Department of Transportation recommended not submitting an application for Safe Routes to School funds this year. He said he did not know if the Florida Department of Transportation was going to award Safe Routes to School grants this year. He added that staff would discuss this issue further with Florida Department of Transportation District 2 staff.

Ms. Scott stated that the City received funding for a Safe Routes to School project. She added that some priorities may receive Transportation Concurrency Exception Area mitigation funding.

Mr. Sanderson stated that the Bicycle/Pedestrian Advisory Board and its staff develop the initial Transportation Alternatives Project fund priorities. He added that the applications would be for sidewalks on one side of the street.

MOTION: Commissioner Hutchinson moved to:

1. **approve the revision of the draft Transportation Alternatives project list to make the Kanapaha Trail project priority number 14;**

2. **approve the submission of two Transportation Alternatives Project applications for construction of the NW 45th Avenue and SW 27th Street/SW 40th Place/SW 25th Terrace sidewalks that are Americans with Disability Act-compliant with sufficient width, if available, so that both sidewalks function as multi-use or combined bicycle/pedestrian facilities; and**

3. **authorize staff to work with City and County staff to identify a Safe Routes to School fund-eligible project and submit an application.**

Commissioner Poe seconded.

FRIENDLY AMENDMENT:

Commissioner Byerly asked to make the Hull Road Connector project priority number 13. Commissioners Hutchinson and Poe accepted the amendment.

MOTION AS AMENDED:

Commissioner Hutchinson moved to:

1. **approve the revision of the draft Transportation Alternatives project list t make the Hull Road Connector project priority number 13 and the Kanapaha Trail project priority number 14 (see Exhibit 1);**

2. **approve the submission of two Transportation Alternatives Project applications for construction of the NW 45th Avenue and SW 27th Street/SW 40th Place/SW 25th Terrace sidewalks that are Americans with Disability Act-compliant with sufficient width, if available, so that both sidewalks function as multi-use or combined bicycle/pedestrian facilities; and**

3. **authorize staff to work with City and County staff to identify a Safe Routes to School fund-eligible project and submit an application.**

Commissioner Poe seconded; motion passed unanimously.

IV. STATE ROAD 226 TRANSPORTATION SYSTEM MANAGEMENT (TSM) PROJECT-60 PERCENT PLANS

Mr. Sanderson stated that the Florida Department of Transportation has submitted 60 percent plans for the SE 16th Avenue Transportation System Management project to the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area for review and comment. He discussed the plans and answered questions.

MOTION: Commissioner Baird move to approve the State Road 226 (SE 16th Avenue) 60 percent plans with a request to try and improve the line-of-sight for northbound traffic on Main Street using the sliplane to go eastbound on SE 16th Avenue and make corresponding adjustments to the sliplane pedestrian crossing to maximize pedestrian safety. Commissioner Pinkoson seconded.

Mr. Sanderson continued to discuss the plans and answer questions.

SUBSTITUTE MOTION:

Commissioner Byerly moved to reject the State Road 226 Transportation System Management 60 Percent Plans. Commissioner Hinson-Rawls seconded; motion passed unanimously.

Several MTPO members discussed the implication of rejecting the plans.

REVISED SUBSTITUTE MOTION:

Commissioner Byerly moved to approve the State Road 226 Transportation System Management 60 Percent Plans with two modifications:

1. **eliminate the sliplane in the southeast corner of the South Main Street and Southeast 16th Avenue intersection in order to increase bicycle, pedestrian and motor vehicle safety; and**

2. **provide a sidewalk that is wider than five feet on the south side of Southeast 16th Avenue in order to accommodate combined-use (bicycle and pedestrian) activity.**

Commissioner Hinson-Rawls seconded; motion passed unanimously.

VIII. COMMENTS

A. MEMBERS

MOTION: Commissioner Chase move to send a letter of apology to the Florida Department of Transportation for the way the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area meeting was cancelled last week. Commissioner Pinkoson seconded; motion passed unanimously.

A member discussed her concerns regarding the roadway conditions and capacity of Tower Road. She also noted that County staff has a simple plan that includes resurfacing and turnlanes with a cost of $8 million with utility modifications and $4 million without the utility modifications.

Mr. Sanderson stated that he was unaware of this plan and that he would contact County staff to get familiar with the plan. He also noted that the MTPO would be considering its List of Priority projects at its next meeting and that this project could be prioritized at that time.

MOTION: Commissioner Baird moved to request information on available funds that can be identified for modifications of Tower Road from Archer Road to Newberry Road. Commissioner Pinkoson seconded. After more discussion, the motion was withdrawn.

A member discussed the County's road maintenance backlog and priority lists. He requested that County staff be available at the next meeting to provide explanation of its project prioritization.

A member noted that the Alachua County Countywide Transportation Summit is scheduled for April 10, 2013 at 5:30 p.m. at the Senior Recreation Center at 5700 NW 34th Street.

It was a consensus that the Summit should be advertised so that it will allow for both Alachua County and City of Gainesville Commissioners to participate in the meeting.

County staff indicated that the meeting would be noticed as requested by the MTPO.

A member noted that the June meeting is scheduled for 5:00 p.m. He said that County Commissioner calendars show a 3:00 p.m. start time.

B. CITIZENS

There were no citizen comments.

ADJOURNMENT

Chair Baird adjourned the meeting at 6:58 p.m.

_____
Date

_____
Lauren Poe, Secretary/Treasurer

Exhibit 1

Table 1A
Bicycle/Pedestrian Priorities - Transportation Alternatives Project Fund
Fiscal Years 2014-15 to 2018-19
(within the Gainesville Metropolitan Area)

Number	Project	Location	Description
1	NW 45 Avenue	FM: NW 13 Street TO: NW 6 Street	Construct ADA-compliant multi-use path
2	SW 27 Street/SW 40 Place/ SW 25 Terrace	FM: SW 35 Terrace TO: Williston Road	Construct ADA-compliant multi-use path
3	E University Avenue [SR 26]	FM: E 9 Street TO: Waldo Road [SR 24]	Pedestrian refuge islands
4	Norton Elementary Trail	FM: NW 39 Avenue TO NW 45 Avenue	Construct bicycle/pedestrian trail
5	NE 15 Street	FM: NE 12 Avenue TO: NE 16 Avenue	Construct ADA-compliant sidewalk
6	NW 2 Street	FM: NW 10 Avenue TO: NW 14 Avenue	Construct ADA-compliant sidewalk
7	Bus Stop Upgrades	AT: RTS Systemwide	Construct bus stops and sidewalk connections
8	SW 40 Boulevard/ SW 47 Avenue	FM: Archer Road TO SW 34 Street	Construct bicycle/pedestrian trail
9	E 10 Street	FM: Depot Avenue Trail TO: NE 3 Avenue	Construct bicycle/pedestrian trail; add refuge island at NE 3 Avenue/ Waldo Road intersection
10 ***Partially Funded***	*SW 43 Street*	*FM: SW 40 Boulevard* *TO: SW 20 Avenue*	*Construct ADA-compliant sidewalk*
11	NW 3 Street	FM: W University Avenue TO: NW 8 Avenue	Construct ADA-compliant sidewalk
12	SW 34 Street Grade-Separated Crossing*	AT: SW 34 Street [SR 121]	Construct bicycle/pedestrian grade-separated crossing
13	Hull Road Connector*	FM: SW 20 Avenue TO: End of Parking Area	Construct bicycle/pedestrian trail
14	Lake Kanapaha Trail	FM: Tower Road TO: Interstate 75	Construct bicycle/pedestrian trail

Note: Projects in italic text are partially funded, as shown in the Transportation Improvement Program.
*2004 Alachua Countywide Bicycle Master Plan Addendum- Archer Braid projects

ADA = Americans with Disabilities Act of 1990; E = East; FM = From; NW = Northwest
RTS = Regional Transit System; SW = Southwest; UF = University of Florida; W = West

Initial Enhancement Priorities were developed by the Bicycle/Pedestrian Advisory Board.

EXHIBIT A

Interested Citizens	Alachua County	City of Gainesville	Florida Department of Transportation
Ed Griffin	Mike Fay	Russ Blackburn	
	Jeff Hays	Paul Folkers	
	Dave Wagner	Teresa Scott	
	Alan Yeatter		

* By telephone
# Spoke and provided written comments

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North Central Florida Regional Planning Council

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2009 NW 67th Place, Gainesville, FL 32653-1603 • 352.955.2200

May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Nondiscrimination Policy Statement

STAFF RECOMMENDATION

Authorize the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area Chair to sign the attached Nondiscrimination Policy Statement.

BACKGROUND

This time each year, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area must sign and submit a Nondiscrimination Policy Statement to the Florida Department of Transportation.

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Nondiscrimination Policy Statement

In compliance with Title VI of the Civil Rights Act of 1964 and other federal, state and local nondiscrimination authorities, it is the policy of the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area that no person shall on the basis of race, color, national origin, sex, age, disability, familial status, religious status, marital status, sexual orientation or gender identity be excluded from participation in, be denied the benefits of, or be otherwise subjected to discrimination or retaliation under any program or activity.

	Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area	
Randy Wells, Chair	Name of Metropolitan Planning Organization	Date

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Auditor Selection Process

STAFF RECOMMENDATION

Recommend the appointment of Commissioner Lauren Poe to represent the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area as a member of the North Central Florida Regional Planning Council Audit Committee.

BACKGROUND

In order to reduce the cost of the audit each year, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area works with the North Central Florida Regional Planning Council to jointly select an auditor to prepare its audit. In the past, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area's Secretary-Treasurer has been recommended to serve on the Planning Council Audit Committee. Therefore, it is recommended that the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area recommend the appointment of Commissioner Lauren Poe to represent the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area on the Planning Council Audit Committee.

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Archer Road at SW 34th Street Intersection Modifications

JOINT RECOMMENDATIONS

The Citizens Advisory Committee, Technical Advisory Committee and staff all recommend approval of the Exhibit 2 Conclusions on page 22.

Note- The Bicycle/Pedestrian Advisory Board discussed this topic and had no recommendation.

BACKGROUND

On June 4, 2012, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved Table 12A Traffic Operations Priorities for the State Highway System (enclosed as Exhibit 1) with one revision to project priority number 4. This revision revised the project description from "Add right turnlanes" to "enhance right turn movement accommodation, such as the accommodation at the SW 34th Street at SW 20th Avenue intersection." The issue that caused this revision was concern about adding additional lanes to an intersection that is already difficult for pedestrians to cross.

Figure 1- Archer Road at SW 34th Street (southbound)

During this discussion, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved a motion to:

> *"request that appropriate staff present at a future meeting possible modifications to accommodate right turn movements at the Archer Road at SW 34th Street intersection."*

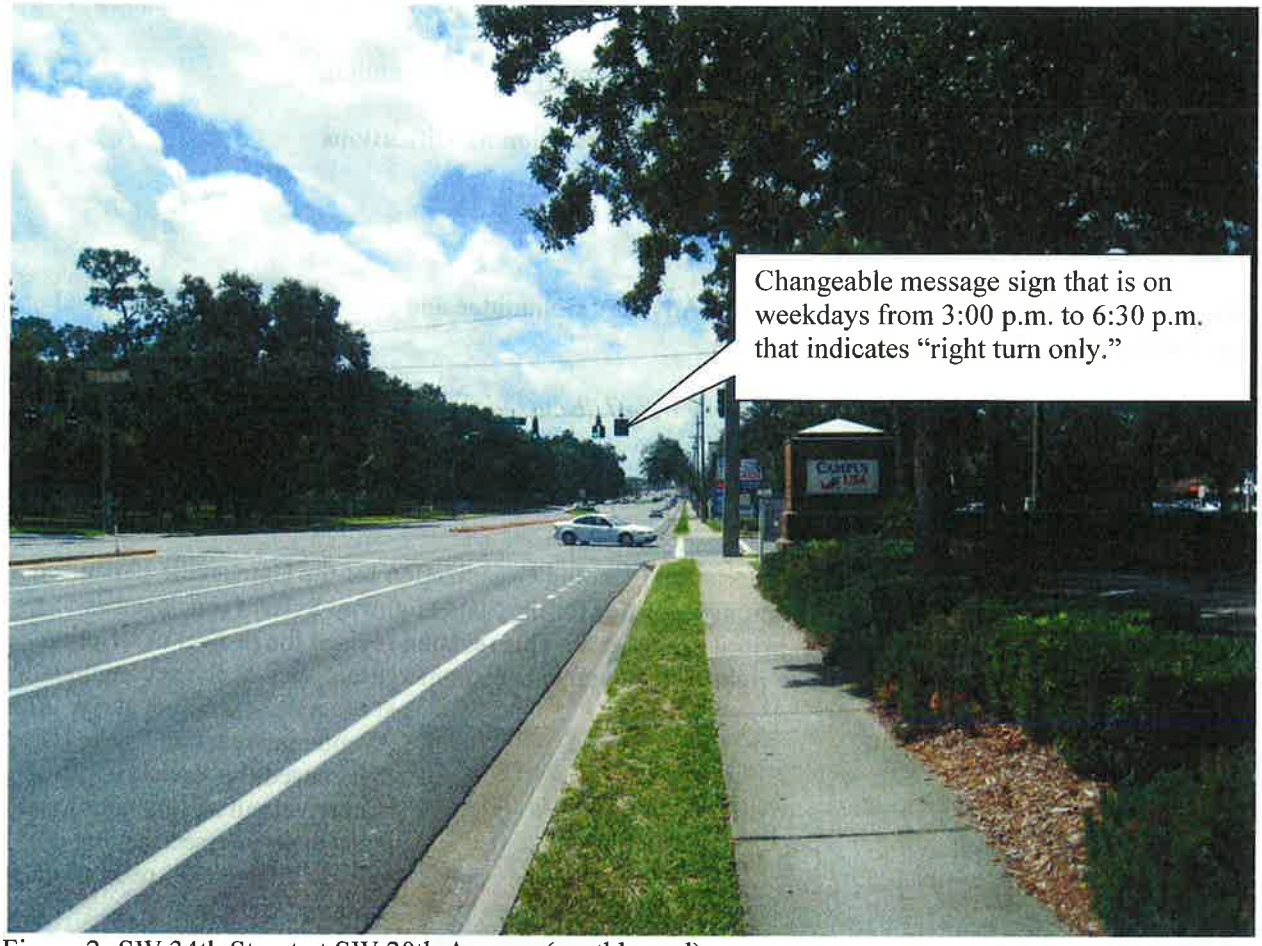

Figure 2- SW 34th Street at SW 20th Avenue (southbound)

Traffic Operations Study

Enclosed as Exhibit 2 is a study prepared by the Florida Department of Transportation District 2 entitled District Wide Traffic Operations Studies Project- Task Order Number: 2- SR 24 (SW Archer Road) at SR 121 (SW 34th Street)- Gainesville Florida.

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EXHIBIT 1

Table 12A
Traffic Operations Priorities - State Highway System Only
Fiscal Years 2013-14 to 2017-18
(within the Gainesville Metropolitan Area)

Number	Project	Location	Description
1	Newberry Road [SR 26]	AT: NW 76 Street	Reconstruction of traffic signal
2	University Avenue [SR 26]	AT: Hawthorne Road [SR 20]	Reconstruction of traffic signal
3	Newberry Road [SR 26]	FM: Ft Clarke Boulevard TO: NW 8 Avenue	Corridor study
4	SW 34 Street [SR 121]	AT: Archer Road [SR 24]	Enhance right turn movement accommodation, such as the accommodation at the SW 34 Street at SW 20 Avenue intersection
5	NW 13 Street [US 441]	AT: NW 6 Street [SR 20]	Intersection realignment/construction/ signalization
6	NW 13 Street [US 441]	FM: 2100 block TO: 2200 block	Access management and pedestrian refuge islands
7 Partially Funded*	*SW 13 Street [US 441]*	*AT: Archer Road [SR 24]*	*Signal reconstruction and retaining wall modifications*
8 Partially Funded	*SW 34 Street [SR 121]*	*AT: Hull Road*	*Extend southbound left turnlane; install northbound right exclusive right turnlane variable message board*
9	SW 34 Street [SR 121]	AT: Radio Road AT: SW 20 Avenue	Traffic signal replacement to address structural deficiency
10	Hawthorne Road [SR 20]	AT: SE 43rd Street	Intersection modification
11	Williston Road [SR 121]	AT: SW 62nd Avenue	Intersection modification
12 Partially Funded	*Archer Road [SR 24]*	*AT: Center Drive / VA Entrance*	*Traffic signal replacement to address structural deficiency*

EXHIBIT 2

TECHNICAL REPORT

District Wide Traffic Operations Studies Project

Contract Number: C9851

Financial Identification Numbers: 432429-1-12-01, 432429-2-12-01, 432429-3-12-01

Task Work Order Number: 2

SR 24 (SW Archer Road) at SR 121 (SW 34th Street) – Gainesville, Florida

Prepared for:

STATE OF FLORIDA • DEPARTMENT OF TRANSPORTATION

FDOT, District 2

Prepared by:

Prosser Hallock

Under Contract to:

ETM
England-Thims & Miller, Inc.
VISION • EXPERIENCE • RESULTS

Submitted by: Fred Kyle, PE, PTOE
Florida PE No. 40360
May 2013

TECHNICAL REPORT

Task Work Order Number: 2

SR 24 (SW Archer Road) at SR 121 (SW 34th Street) – Gainesville, FL

INTRODUCTION:

Prosser Hallock, Inc. (PH) under contract to England-Thims & Miller, Inc. (ETM) was tasked by the Florida Department of Transportation with analyzing the intersection of SR 24 (SW Archer Road) and SR 121 (SW 34th Street) in Gainesville, Florida, and providing recommendations to improve traffic flow for motorists at this intersection. The intersection of SR 121 and Windmeadows Boulevard was also included in the study area because of its close proximity to the subject intersection. The focus of this task was to examine the request of the Gainesville Metropolitan Transportation Planning Organization (MTPO) to "Enhance the right turn movement accommodation, such as the accommodation at the SW 34th St. at SW 20th St. intersection." Although the focus of this study was the north approach right turn movement, other capacity/operational improvements at this intersection were also identified and analyzed.

Currently, the intersection of SR 24/SW Archer Road and SR 121/SW 34th Street is a signalized intersection with three through lanes and two left turn lanes on each approach. In addition, right turn channelization islands exist for the right turn movements on SR 24. The signalization at these intersections includes fully protected left turn phases that either lead or lag the through movements by time-of-day.

Currently, speed limits of 45 mph exist on both roadways.

Intersection of SR 24 (SW Archer Road) and SR 121 (SW 34th Street)

The intersection of SR 121 and Windmeadows Boulevard is approximately 550 feet north of the SR 24 intersection. This "tee" intersection is also signalized. SR 121 has three lanes in each direction with a short left turn lane on the south approach for vehicles turning on Windmeadows Boulevard. Windmeadows Boulevard is a basic two lane road that widens to provide three approach lanes at the intersection – two left turn lanes and a single right turn lane. The speed limit on Windmeadows Boulevard is 25 mph.

SR 121 (SW 34th Street) and Windmeadows Boulevard

As can be seen from these aerial pictures, the land uses around these two intersections is primarily retail commercial. A large shopping center with several out parcels is located on the northwest quadrant of the SR 24/SR 121 intersection. Smaller individual retail businesses and small strip retail centers occupy the other three quadrants. In addition, several large multi-family residential developments are in close proximity to these intersections. Windmeadows Boulevard provides a back access to this large shopping center, as well as access to several of the multi-family developments. The University of Florida campus is north and east of these intersections.

The traffic signals at these two intersections are part of large coordinated signal systems on SR 24 and on SR 121. The SR 24/SR 121 intersection is the critical intersection in both of these systems. The traffic signal at Windmeadows Boulevard is cross-coordinated with the SR 24 intersection to provide coordinated operation on SR 121.

Figure 1 provides an overall view of the study area including the large retail commercial center in the northwest quadrant of the SR 24/SR 121 intersection.

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

ProsserHallock

STATE OF FLORIDA - DEPARTMENT OF TRANSPORTATION

ROAD NO	COUNTY	FINANCIAL PROJECT ID
SR 121	ALACHUA	—

TRAFFIC OPERATIONS STUDY
STUDY AREA

FIGURE NO 1

ETM was asked by the Department to identify improvements to the intersection that would improve traffic flow in the area. Of particular interest was the MTPO's request to enhance the right turn movement by converting the through/right lane on the north approach of the SR 24/SR 121 intersection to an exclusive right turn lane either full time or by time-of-day. Although the primary focus of this study was the north approach right turn movement, other potential intersection improvements were also studied.

North Approach of the SR 121/SR 24 intersection looking south toward SR 24

DATA COLLECTION:

Turning movement counts were made at both intersections. These counts, copies of which are included in the Appendix, were made from 7:00 A.M. to 9:00 A.M., 12:00 P.M. to 2:00 P.M., and 3:00 P.M. to 7:00 P.M. on January 29, 2013. Copies of the turning movement counts are included in the Appendix.

Traffic signal timing data for both intersections was obtained from the City of Gainesville Traffic Management Center. The timing data not only included phase split times for the various traffic

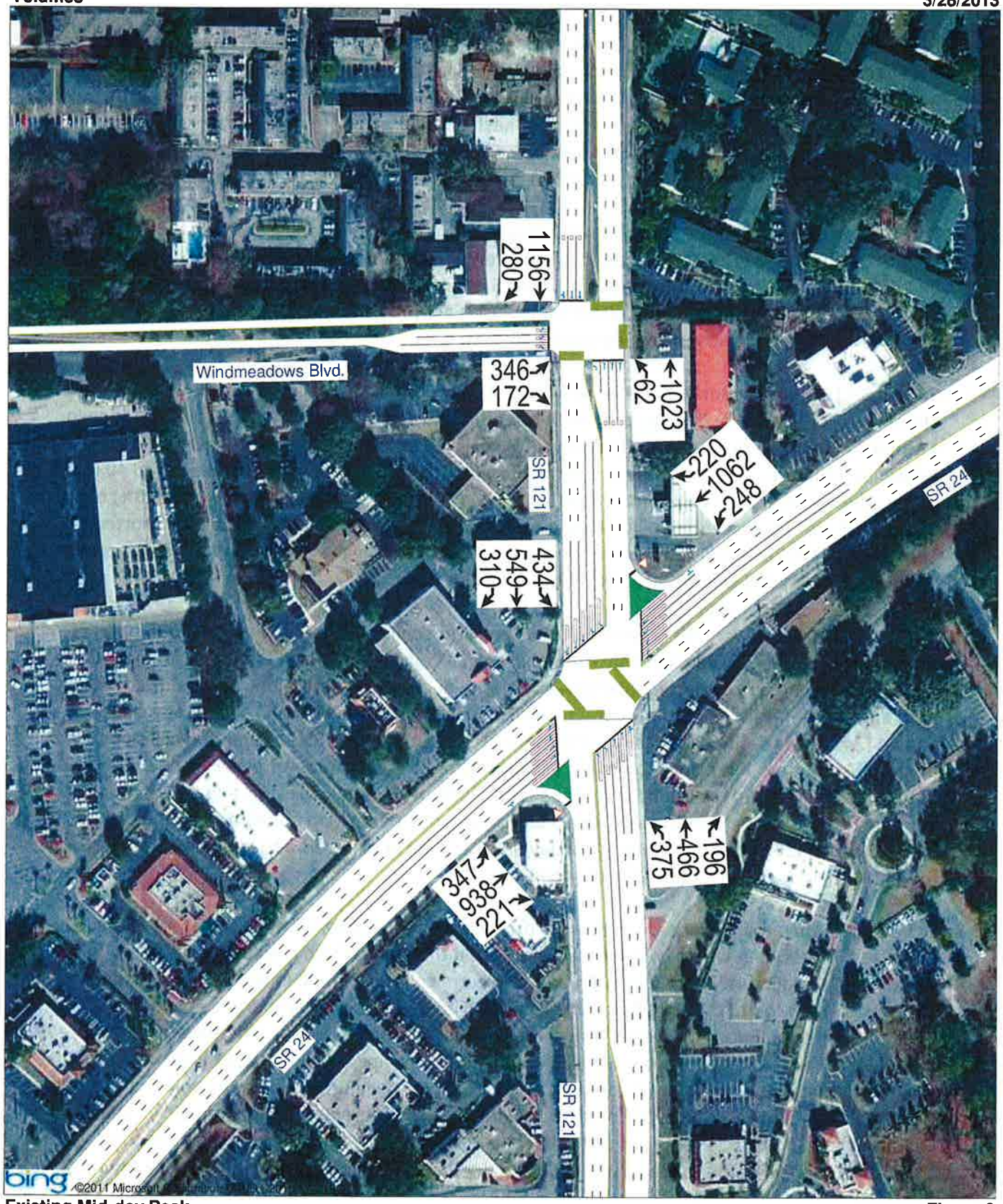
plans in effect at these intersections, but the phase sequencing as well. This is critically important because the left turn movements at both intersections lead or lag the through movements based on the specific timing plan in effect. The traffic volume data and signal timing data were used as inputs into the Synchro/SimTraffic software package. The Synchro/SimTraffic results were then used to compare Measures of Effectiveness (MOEs) for each considered alternative. Copies of the signal timing sheets are also included in the Appendix. Figures 2, 3, and 4 provide a graphical representation of the A.M., Mid-day, and P.M. peak hour volumes used in this analysis.

The FDOT provided all of the Long Form Crashes found in the CAR database for the study area from 1/1/09 through 12/31/11. Most of the collisions centered around the two signalized intersections of SR 24/SR 121 and SR 121/Windmeadows. Figure 5 is a collision diagram showing the various collision types and the locations. Collision summaries are provided in the Appendix.
The following is a summary of the reported collisions:

SR 24 / SR 121 Intersection – There were 147 total Long Form collisions found in the CAR database. There were 112 property damage only collisions. Thirty five collisions involved injuries resulting in 47 injuries. There were no reported fatalities. A majority of the collisions were rear-end or sideswipe collisions. Rear end collisions accounted for 101 (69%) of the total collisions and 40 (85%) of the injuries. Sideswipe collisions accounted for 24 (16%) of the total collisions and 3 (10%) of the injuries. There was one collision involving a bicycle and no pedestrian collisions were reported. There were 112 (76%) collisions during the day and 35 (24%) at night. The roadway was reported dry for 127 (86%) and wet for 20 (14%) of the collisions.

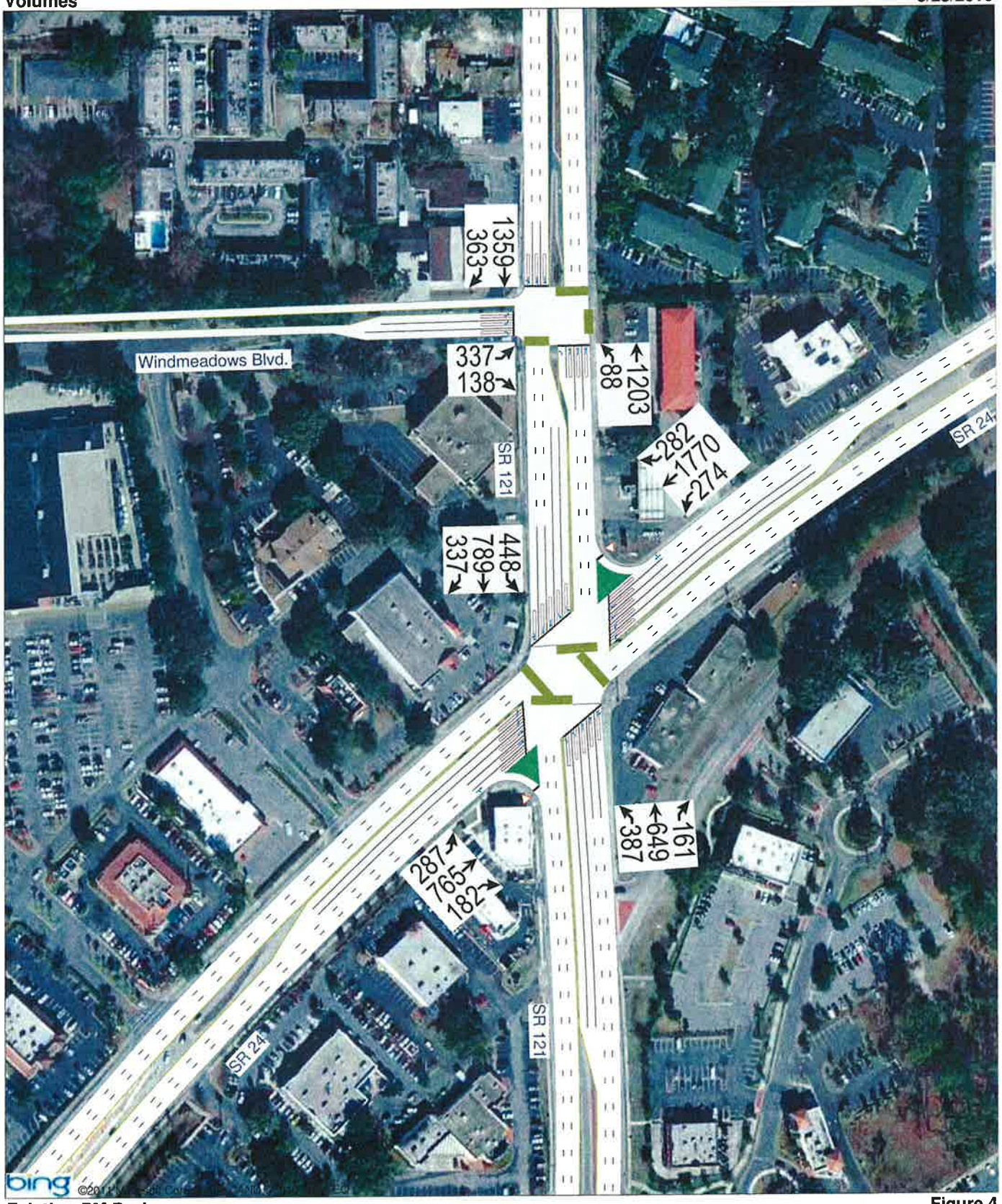
SR 121 / Windmeadows – There were 59 total Long Form collisions found in the CAR database, of which 38 were property damage only collisions, and 21 collisions involved injuries resulting in 28 injuries. There were no reported fatalities. A majority of collisions were either rear-end, sideswipe, right angle, or left turn collisions. Rear end collisions accounted for 26 (44%) of the total collisions and 14 (24%) of the injuries. Sideswipe collisions accounted for 10 (18%) of the total collisions and 3 (4%) of the injuries. Right angle collisions accounted for 13 (22%) of the total collisions and 4 (14%) of the injuries. Left turn collisions accounted for 6 (10%) of the total collisions and 6 (21%) of the injuries. There was one collision involving a bicycle and one pedestrian collision was reported. There were 45 (76%) collisions during the day and 12 (24%) at night. The roadway was reported dry for 48 (81%) and wet for 11 (19%) of the collisions.

Driveways in the area – There are numerous driveways within approximately 1,000 feet of the signalized intersection where collisions were reported that appear to be related to delays at the signalized intersections. There were 10 total Long Form collisions found in the CAR database that appear to be related to the signalized intersections. There were seven property damage only collisions. Three collisions involved injuries, resulting in a total of 4 injuries. There were no reported fatalities. The majority of collisions were right angle or sideswipe collisions. Right angle collisions accounted for 5 (50%) of the total collisions and 2 (50%) of the injuries. Sideswipe collisions accounted for 2 (20%) of the total collisions and no injuries. There was one collision involving a pedestrian and no bicycle collisions were reported. There were 7 (70%) collisions during the day and 3 (30%) at night. The roadway was reported dry for all 10 (100%) and wet for none of the collisions.



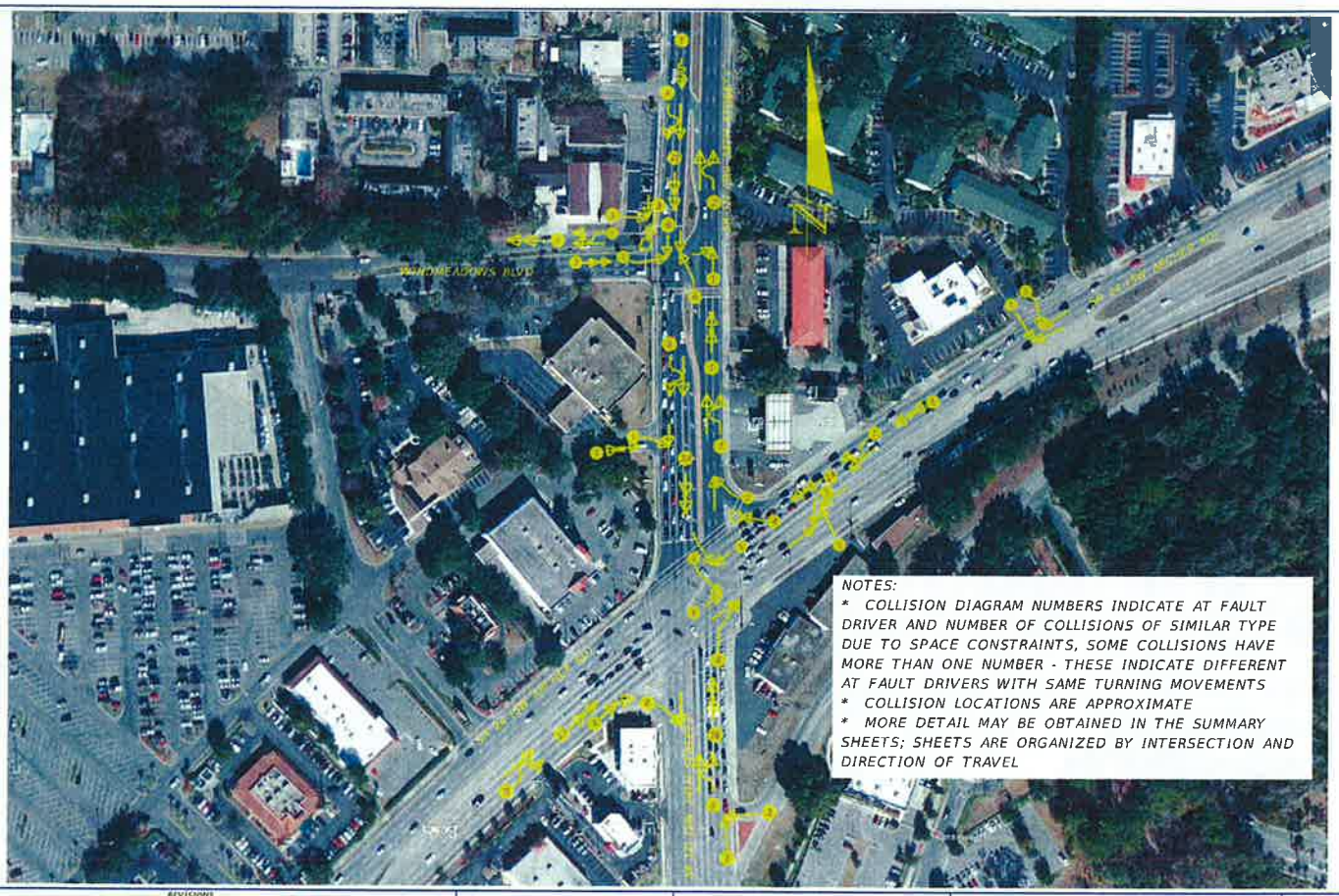
Existing Mid-day Peak
Prosser Hallock, Inc.

Figure 3



Existing PM Peak
Prosser Hallock, Inc.

Figure 4



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

Prosser Hallock

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

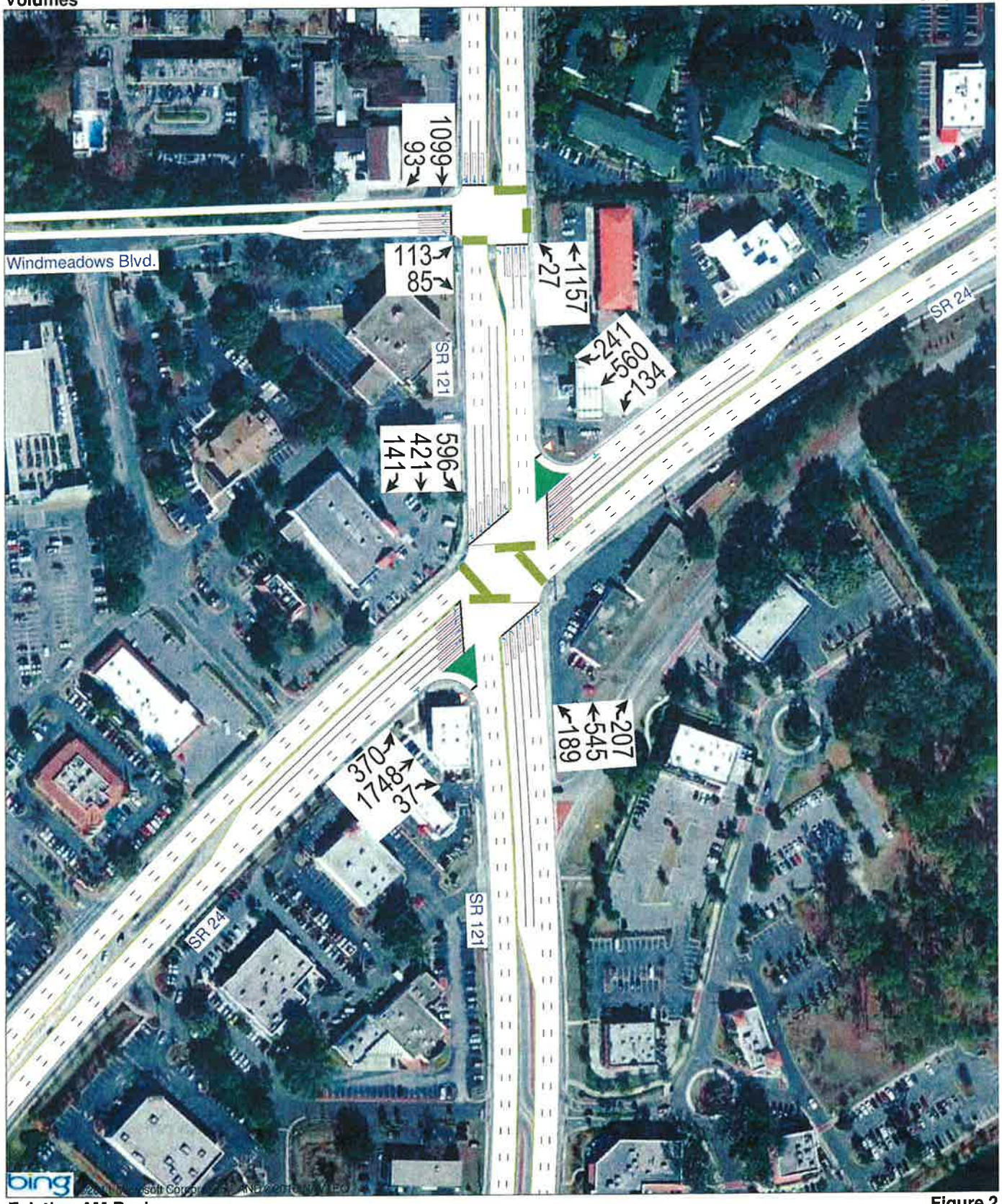
ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 121	ALACHUA	—

TRAFFIC OPERATIONS STUDY
COLLISION DIAGRAM
1/1/09 - 12/31/11

FIGURE NO 5

Map - SR 24 & SR 121 Operational Analysis
Volumes

SR 24 & SR 121 Operational Analysis
3/28/2013



Existing AM Peak
Prosser Hallock, Inc.

Figure 2

Summary of all collisions – There were 216 total Long Form collisions found in the CAR database. One hundred fifty-seven of these collisions were property damage only. Fifty-nine collisions involved injuries, with a total of 79 injuries. There were no reported fatalities. A majority of the collisions were rear end or sideswipe collisions. Rear end collisions accounted for 129 (60%) of the total collisions and 56 (71%) of the injuries. Sideswipe collisions accounted for 34 (16%) of the total collisions and 6 (8%) of the injuries. There were two collisions involving a bicycle and two pedestrian collisions reported. There were 164 (76%) collisions during the day and 52 (24%) at night. The roadway was reported dry for 185 (86%) and wet for 31 (14%) of the collisions.

OBSERVATIONS:

Site visits to observe traffic operations at these intersections were made on February 27 and 28, 2013. Some of the issues observed are as follows:

- Morning peak period
  - north approach left turn queues on SR 121 at SR 24 often extended north of Windmeadows Boulevard
  - west approach through queues on SR 24 extended almost to SW 35th Boulevard, but cleared each signal cycle
  - other movements cleared the intersections each signal cycle
  - pedestrian activity along SR 24 resulted in the possibility of pedestrian calls most cycles
- Mid-day peak period
  - north approach right turn volumes at the SR 24/SR 121 intersection are heavy
  - north approach right turn volumes at the SR 121/Windmeadows intersection are heavy
  - north approach through volumes at both intersections are also heavy
  - the right-in/right-out commercial driveway on SR 121 between Windmeadows and SR 24 that serves the retail commercial development is heavily used with many of the exiting vehicles continuing south on SR 121
  - east approach right turn volumes on SR 24 at SR 121 are heavy
  - pedestrian activity along SR 24 resulted in the possibility of pedestrian calls most cycles
- Afternoon peak period
  - east approach queues on SR 24 extend over 5,000 feet to the east, taking 3-4 cycles to reach the SR 121 intersection
  - north approach through and left turn queues are also heavy, extending north of Windmeadows Boulevard
  - the right-in/right-out commercial driveway on SR 121 between Windmeadows and SR 24 that serves the retail commercial development is heavily used, with most of the vehicles continuing south on SR 121
  - pedestrian activity along SR 24 resulted in the possibility of pedestrian calls most cycles

In the course of traveling to and from the study area, the team also briefly observed the intersection of SW 34th Street and SW 20th Street. These observations were made between 4:30 P.M. and 4:45 P.M.; at a time when the right turn only restriction was in effect. During the brief visit to this intersection we observed a Gainesville Police Department officer parked over the curb near the intersection for the purpose of enforcing the right turn only restriction. Our team was there only a few minutes when a violation occurred and the officer left the scene and was later observed ticketing the offending driver. While the officer was away, a number of violations of the right turn restriction were observed.

ALTERNATIVES:

The focus of this task work order was to investigate the possibility of restriping the outside lane of the north approach of the intersection of SR 24 and SR 121. Currently this lane is striped as a through/right turn lane. As noted in the field observations, and the turning movement counts, the north approach right turn volumes at this intersection are quite heavy from mid-day through the P.M. peak resulting in vehicle queuing in the outside lane. The goal of this study was to determine if converting this lane to a right turn only lane either full time or only during the peak periods will reduce vehicle queues and improve intersection efficiency. Of note, this alternative was analyzed with and without a right turn overlap signal phase.

Constructing a separate right turn lane was also considered. However, in order to construct the right turn lane, additional right-of-way would be needed from the CVS Pharmacy located in the northwest corner of the intersection. It appears that the right-of-way line is located at the back of sidewalk, which is about 8' from the back of curb. In places, the CVS parking lot is only about 11' from the back of curb. Therefore, adding a right turn lane would cause the pharmacy to lose several parking places.

In addition, underground utilities such as water (a fire hydrant is located on the corner), underground electric service for the streetlights, and underground phone ducts (as evidenced by a large switch cabinet) are evident just behind the sidewalk. Also, the traffic signal strain pole on this corner supporting the signal span is located in the back of the sidewalk. If a right turn lane is added, this concrete stain pole would need to be relocated resulting in a complete rebuild of the traffic signal.

Due to limited available right-of-way, the possibility of high business-damage costs (resulting from the loss of private property as well as the loss of existing parking spaces), numerous utility conflicts, and the need to replace the existing traffic signal, adding a separate right turn lane was not analyzed further.

Our site observations also revealed that the north approach left turn queues on SR 121 at SR 24 frequently extended beyond the SR 121/Windmeadows Boulevard intersection during the A.M. and P.M. peak periods. As a result, we analyzed the effects of extending one of the southbound left turn lanes north of the Windmeadows Boulevard intersection.

During our site observations and discussions with City of Gainesville Traffic Management Center staff, it became obvious that improvements to the east approach of SR 24 would also be helpful. As mentioned previously, significant queuing occurs during the P.M. peak with queues measuring over 5,000 feet long. Since this approach currently has three through lanes and two left turn lanes, we also analyzed the benefits of adding a dedicated right turn lane at this intersection. Based on our field reviews, it appears that sufficient right-of-way exists to add this additional turn lane to this approach.

Synchro/SimTraffic software was used to develop Measures of Effectiveness (MOEs) for comparing the alternatives to the existing conditions. Inputs used in the analysis included the existing traffic volumes and the current traffic signal timing. Since these two intersections are a part of larger coordinated signal systems, new signal timing was not developed. The following alternatives were analyzed:

- Existing Conditions
- Alternate 1
    - add a right turn lane to the east approach of SR 24,
    - restripe the outside lane of the north approach of SR 121 creating a right turn only lane, resulting in a right turn only lane, two through lanes, and two left turn lanes on this approach, and
    - lengthen one of the north approach left turn lanes to extend north of the Windmeadows intersection.
- Alternate 2
    - Includes Alternate 1 options plus a right turn overlap signal phase for the north approach of the SR24/SR 121 intersection.

Figures 6 – 8 provide graphical representations of the analyzed improvements to SR 121 and SR 24.

It should be mentioned that Synchro is a macroscopic model that represents traffic in an aggregate measure for the time period analyzed. SimTraffic is a microscopic model that individually tracks every vehicle through the network during each 0.1 second of simulation. These differences are important when dealing with over-saturated conditions or conditions where queues extend upstream to the next signalized intersection. SimTraffic provides MOEs for every vehicle during the simulation and better reflects the impacts of oversaturation and downstream roadway conditions on driver behavior.

Tables 1, 2, and 3, contain a summary of the key Synchro Measures of Effectiveness using the existing traffic volumes and Tables 4, 5, and 6 contain a summary of the key SimTraffic Measures of Effectiveness. The Synchro and SimTraffic reports are included in the Appendix.

The analysis was performed using traffic volume and signal timing data for the morning peak, the noon or mid-day peak and the afternoon peak. The specific hours of analysis were from 7:30 A.M. – 8:30 A.M., 12:15 P.M. – 1:15 P.M., and 4:30 P.M. – 5:30 P.M. These were the hours when the traffic volumes were the highest. The Synchro and SimTraffic results for each time period indicate that while the conversion of the north approach outside lane helps the right turn traffic, the delay and

REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

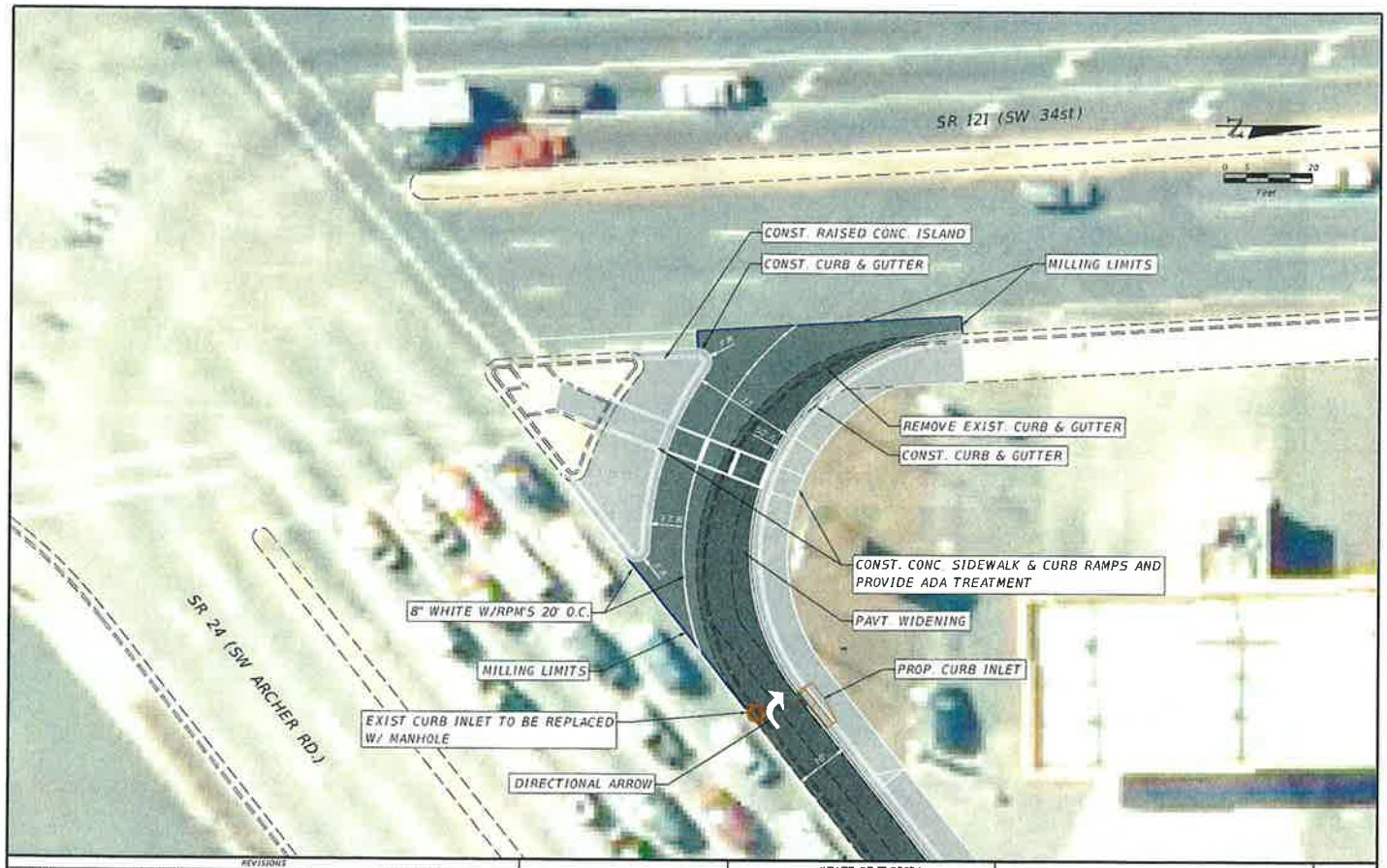
ETM

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 115	DUVAL	432429-1-32-01

TRAFFIC OPERATIONS STUDY
SR 24 (SW ARCHER RD.) &
SR 121 (SW 34TH ST.)

FIGURE 6



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

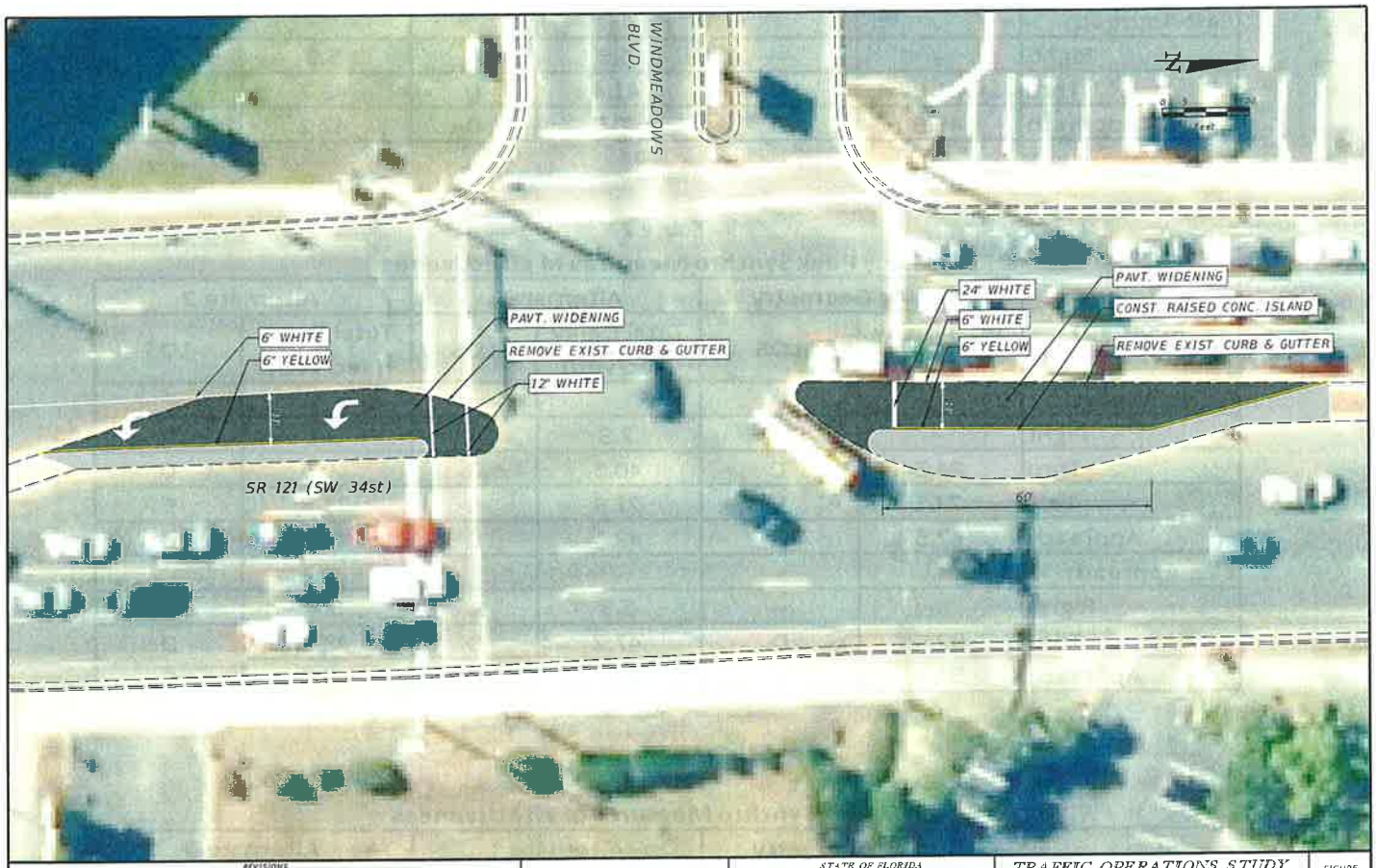
ETM

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 134	DUVAL	432429-1-32-01

TRAFFIC OPERATIONS STUDY
SR 24 (SW ARCHER RD.) &
SR 121 (SW 34TH ST.)

FIGURE
2



REVISIONS			
DATE	DESCRIPTION	DATE	DESCRIPTION

ETM

STATE OF FLORIDA
DEPARTMENT OF TRANSPORTATION

ROAD NO.	COUNTY	FINANCIAL PROJECT ID
SR 134	DUVAL	432429-1-32-01

TRAFFIC OPERATIONS STUDY
WINDMEADOWS BLVD. &
SR 121 (SW 34TH ST.)

FIGURE

Table 1
A.M. Peak Synchro Measures of Effectiveness

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	9.4	A	10.1	B
Thru	44.8	D	51.1	D	51.1	D
Left	142.0	F	142.4	F	142.4	F
Approach	94.8	F	93.0	F	93.1	F
East Approach						
Right	--	--	5.6	A	5.8	A
Thru	41.3	D	41.4	D	43.7	D
Approach	49.9	D	40.5	D	42.2	D

Table 2
Mid-day Peak Synchro Measures of Effectiveness

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	7.3	A	9.1	A
Thru	34.5	C	40.6	D	40.6	D
Left	73.1	E	71.8	E	71.8	E
Approach	47.5	D	43.1	D	43.5	D
East Approach						
Right	--	--	5.8	A	5.8	A
Thru	52.6	D	47.2	D	48.1	D
Approach	55.4	E	44.9	D	45.5	D

Table 3
P.M. Peak Synchro Measures of Effectiveness

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	34.7	C	34.4	C
Thru	94.3	F	110.3	F	110.3	F
Left	77.0	E	78.9	E	78.9	E
Approach	89.4	F	85.2	F	85.1	F
East Approach						
Right	--	--	9.8	A	9.8	A
Thru	135.2	F	75.0	E	75.0	E
Approach	128.4	F	67.5	E	67.5	E

Table 4

A.M. Peak SimTraffic Measures of Effectiveness

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	25.0	1.0	9	76	158	5.4	0.2	18	3	15	4.3	0.2	19	2	14
Thru	42.9	5.8	7	193	472	43.1	5.7	6	110	190	43.4	5.8	6	117	207
Left	89.1	15.4	3	284	388	89.2	15.2	3	295	418	89.6	15.3	3	294	408
East															
Right	11.9	0.9	36	166	306	6.0	0.4	39	8	73	6.4	0.5	39	8	74
Thru	39.3	6.9	26	133	217	37.8	6.5	27	116	180	40.8	7.1	26	128	196
Intersection	77.3	122.5	11	NA	NA	77.8	122.0	11	NA	NA	76.3	120.4	11	NA	NA

Table 5

Mid-day Peak SimTraffic Measures of Effectiveness

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	23.8	2.1	9	80	188	9.8	0.9	14	21	66	7.4	0.7	15	18	66
Thru	33.2	5.9	8	66	218	33.2	5.9	8	94	184	31.9	5.7	8	91	180
Left	90.9	12.1	3	183	304	75.2	9.7	4	161	286	72.1	9.2	4	154	267
East															
Right	30.0	2.0	28	306	425	8.4	0.5	38	8	91	8.1	0.5	38	2	40
Thru	51.7	16.7	23	266	381	48.0	15.7	24	237	327	46.3	15.0	25	229	309
Intersection	49.1	79.0	16	NA	NA	46.0	74.6	17	NA	NA	44.4	71.7	17	NA	NA

Table 6

P.M. Peak SimTraffic Measures of Effectiveness

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	66.2	7.1	4	342	423	23.7	2.5	9	107	232	19.6	2.0	10	96	224
*Thru	104.9	32.3	4	629	1028	173.6	52.1	3	794	1343	171.1	51	3	762	1278
Left	94.4	13.3	3	246	418	70.5	9.4	4	174	305	69.2	9.3	4	180	301
East															
Right	361.6	31.1	6	2257	3852	33.6	2.8	27	294	593	44.7	3.6	24	320	597
Thru	317.8	183.3	6	2238	3848	76.5	43.4	19.0	574	851	86.7	50.2	17	670	1020
Intersection	171.9	345.4	6	NA	NA	84.2	166.2	12	NA	NA	83.8	166.8	12	NA	NA

*North Approach queues extend north of Windmeadows Blvd. Delays and queues shown include those for Windmeadows.

queues increase for the through traffic. This is to be expected since the number of through lanes is reduced from three lanes to two.

In regards to the implementation of a right turn only restriction by time-of-day, this would typically be done to relieve congestion during the peak periods. Since this study analyzed the effects of a right turn lane during the peak hours, a time-of-day implementation to address peak hour conditions is not recommended for the same reasons as mentioned previously. In addition, time-of-day implementation would require significant enforcement to insure driver compliance.

These results also indicate that there is a benefit to lengthening one of the north approach left turn lanes. These benefits come from providing more queue storage for left turn vehicles so they do not block the through lanes. A benefit also results from the separating the left turn and through vehicles so the vehicle headways are shorter resulting in less delay. These benefits are especially noticeable in the SimTraffic analyses.

FUTURE VOLUMES

A final step in the study included estimating future traffic volumes and comparing the alternatives under future conditions. The FDOT 2011 Florida Transportation Information data disk contains historic data for traffic counts made on SR 24 east and west of SR 121 and on SR 121 north and south of SR 24. Trends analysis software was used to develop traffic volume growth rates to estimate future volumes. The Trends software projected a very minimal or negative growth rate for these volumes; therefore, a 1% growth rate was used to develop future volumes. After discussions with Department staff, a minimum 20-year horizon was chosen for the future analyses. With this guideline, 2035 was chosen as the horizon year. Future volumes were developed and used in the Synchro/SimTraffic analyses. The future analyses also included the development of new traffic signal timing for the two signalized intersections. Copies of the Trends analysis are included in the Appendix.

Since oversaturated conditions currently exist, the future analyses yielded results that are similar to the current volume analysis, just with a difference in magnitude.

Tables 7, 8, and 9 contain the Synchro results with the future volumes and Tables 10, 11, and 12 summarize the results of the SimTraffic future analyses.

Table 7
A.M. Peak Synchro Measures of Effectiveness
Future Volumes

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	4.7	A	8.3	A
Thru	43.6	D	50.1	D	50.1	D
Left	276.6	F	277.1	F	277.1	F
Approach	163.5	F	161.4	F	161.8	F
East Approach						
Right	--	--	6.1	A	6.1	A
Thru	51.3	D	47.2	D	47.3	D
Approach	66.6	E	52.5	D	52.6	D

Table 8
Mid-day Peak Synchro Measures of Effectiveness
Future Volumes

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	17.4	B	14.2	B
Thru	39.1	D	44.7	D	44.7	D
Left	98.6	F	98.4	F	98.4	F
Approach	59.1	E	56.2	E	55.4	E
East Approach						
Right	--	--	5.4	A	5.4	A
Thru	75.6	E	51.6	D	51.6	D
Approach	77.8	E	51.0	D	51.0	D

Table 9
P.M. Peak Synchro Measures of Effectiveness
Future Volumes

Approach	Existing Geometry		Alternate 1		Alternate 2	
	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS	Total Delay (sec/veh)	LOS
North Approach						
Right	--	--	58.8	E	36.3	D
Thru	150.2	F	154.1	F	154.1	F
Left	315.7	F	315.7	F	315.7	F
Approach	197.3	F	179.7	F	174.8	F
East Approach						
Right	--	--	11.7	B	11.7	B
Thru	190.7	F	109.8	F	109.8	F
Approach	177.9	F	94.6	F	94.6	F

re.

Table 10
A.M. Peak SimTraffic Measures of Effectiveness
Future Volumes

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	59.3	2.5	5	124	245	8.4	0.4	15	9	40	5.4	0.2	18	3	19
*Thru	212.4	61.5	5	1707	2529	216.9	62.4	6	1470	2359	226.7	65.6	6	1554	2314
Left	152.8	26.5	2	349	352	158.0	27.1	2	421	467	159.1	27.2	2	423	464
East															
Right	18.8	1.6	32	238	363	7.5	0.7	38	20	119	7.6	0.7	38	22	127
Thru	44.4	9.8	25	182	280	41.3	9.1	26	159	227	43.3	9.7	25	163	233
Intersection	154.0	279.7	6	NA	NA	163.1	295.3	6	NA	NA	157.7	286.7	6	NA	NA

*North Approach queues extend north of Windmeadows Blvd. Delays and queues shown include those for Windmeadows.

Table 11
Mid-day Peak SimTraffic Measures of Effectiveness
Future Volumes

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	46.7	4.8	6	189	342	18.6	2.0	10	72	163	10.8	1.1	13	45	122
*Thru	156.2	55.5	6	1813	2024	141.4	52.1	6	1637	2053	134.3	47.9	6	943	2046
Left	172.4	25.8	2	329	412	178.3	27.0	2	390	473	169.8	25.8	2	376	340
East															
Right	124.3	9.9	14	747	1249	15.1	1.2	34	123	425	15.6	1.3	34	130	437
Thru	131.1	53.2	13	712	1232	66.8	27.5	20	375	596	67.5	27.7	20	376	618
Intersection	114.4	226.8	9	NA	NA	92.2	183.7	10	NA	NA	87.0	171.3	11	NA	NA

*North Approach queues extend north of Windmeadows Blvd. Delays and queues shown include those for Windmeadows.

Table 12
P.M. Peak SimTraffic Measures of Effectiveness
Future Volumes

Approach	Existing Geometry					Alternate 1					Alternate 2				
	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)	Total Delay/Veh (sec/veh)	Total Delay (Hr)	Avg. Speed (mph)	Avg. Queue (ft)	95% Queue (ft)
North															
Right	56.3	5.1	5	189	396	43.5	4.1	6	105	223	35.3	3.3	7	83	186
*Thru	284.9	97.5	5	1878	2412	345.0	113.9	3	1802	2482	352.6	116.3	3	1795	2502
Left	265.6	31.0	1	349	352	264.5	31.2	1	426	476	259.3	31.0	1	427	474
East															
Right	455.6	42.5	4	3335	4688	223.2	22.5	9	355	583	192.2	19.3	10	335	594
Thru	417.9	263.5	5	3331	4704	230.6	165.8	8.0	2163	3781	213.1	151.8	9	1978	3706
Intersection	288.1	571.7	4	NA	NA	216.5	450.6	6	NA	NA	209.7	432.2	6	NA	NA

*North Approach queues extend north of Windmeadows Blvd. Delays and queues shown include those for Windmeadows.

RECOMMENDATIONS:

Based on the Synchro analysis, restriping the outside lane of the north approach of SR 121 to only serve right turn movements does not appear to be justified because of negative impacts to other intersection movements. While restriping the north approach will provide a separate right turn lane, the number of through lanes will be reduced from three lanes to two lanes, resulting in a reduction in operational efficiency on this approach.

As shown in Tables 1 and 3, the delay per through vehicle on this approach in the morning peak increases approximately 14%, from 44.8 seconds per vehicle to 51.1 seconds per vehicle. During the afternoon peak this delay increases 17% (94.3 seconds per vehicle to 110.3 seconds per vehicle).

The impacts of implementing a dedicated right-turn lane are also reflected in the SimTraffic micro-simulation results. During the afternoon peak period, the delay to the through vehicles is increased by 65%, from 104.9 seconds to 173.6 seconds. Not only is the delay increased, but the vehicle queues are also increased, from 1028 feet to 1343 feet (31%).

It should be pointed out that providing a separate right turn lane will indeed reduce the delay to right turn traffic since motorists making this movement would have exclusive use of the right lane. The SimTraffic results for the afternoon peak show a reduction in delay from 66.2 seconds per vehicle to 23.7 seconds per vehicle, a 64% reduction.

In addition to the increased delay and vehicle queues for the through movements, modifying the outside lane has other disadvantages. First, during the field observations, a relatively large number of vehicles were observed exiting the right-in/right-out driveway that is located on SR 121 between SR 24 and Windmeadows Boulevard. Most of these vehicles entered the outside lane and proceeded south through the SR 24/SR 121 intersection. If the outside lane becomes a right turn only lane, these vehicles will need to cross the right turn lane in order to enter a through lane, resulting in increased vehicle conflicts.

Second, restriping the outside lane will require the relocation of the existing bicycle lane that exists along SR 121. While FDOT Standard Index 17347 provides guidance to accomplish this transition, cyclists will be required to cross the right turning traffic in order to stay in the bicycle lane.

Finally, implementing this change would disrupt lane continuity on SR 121. The six-lane section of SR 121 begins just north of W. University Avenue, which is about 1 ¾ miles north of SR 24 and continues to SE Williston Road, a distance of approximately 1.6 miles south of SR 24. Converting the outside lane at SR 24 would eliminate lane continuity in the outside lane resulting in numerous lane changes, increasing the number of vehicle conflict points thus, potentially increasing the crash frequency.

As mentioned previously in this report, implementation of a right turn only restriction by time-of day would typically be done to relieve congestion during the peak periods. Since this study analyzed the effects of a right turn lane during the peak hours, a time-of-day implementation to address peak

hour conditions is not recommended for the same reasons as mentioned previously. In addition, time-of-day implementation would require significant enforcement to insure driver compliance.

In summary, the analysis shows that restriping the north approach of the SR 24/SR 121 intersection to provide a right turn lane and two through lanes either permanently or by time-of-day will slightly reduce the overall north approach delay and the through movement delay during the A.M. and Mid-day peak periods, but the approach delay and through movement delay is greatly increased during the P.M. peak period. Because the disadvantages of restriping the outside lane outweigh the advantages gained by the right-turn movement, it was determined that this improvement should not be recommended.

In addition to estimating the impacts of restriping the north approach of the SR 24/SR 121 intersection, the Synchro and SimTraffic analyses were used to identify other improvements that might be considered. The greatest improvement to traffic flow is expected to occur with the construction of a right-turn lane on the east approach of SR 24 at the SR 121 intersection. This improvement is expected to substantially reduce the existing queues and delays on the east approach, especially during the P.M. peak period. Lengthening the outside left turn lane on the north approach of SR 121 at the SR 24 intersection will also improve traffic operations at this location by providing additional storage for the left turning vehicles.

CONCEPTUAL PLANS – OPINION OF PROBABLE COSTS:

Conceptual plans highlighting the recommended improvements are provided for the Department's consideration (please see Figures 6-8). Based on these concepts, it appears a reasonable cost for these improvements is approximately $230,000. This estimate includes $30,000 for project unknowns and a 30% contingency (because these are relatively-small improvements and historical unit-cost prices may not apply). Also, this opinion does not include any right-of-way costs that may be needed (to reconstruct the proposed right-turn radius on the northeast corner). A detailed cost estimate is included in the report's Appendix.

CONCLUSION:

Based on the Synchro/SimTraffic analyses and our site investigations, restriping the outside lane of the north approach of the SR 24/SR 121 intersection to form a right turn only lane would reduce delay to the right turn movement. However, the delay to the north approach would be increased since the number of through lanes would be reduced from three lanes to two. In addition, the through traffic on this approach is expected to queue beyond the Windmeadows Boulevard intersection.

This study also identified other improvements that could improve traffic operations at the intersection. Constructing a right turn lane on the east approach of SR 24 at the SR 121 intersection will greatly reduce delay and vehicle queues, especially during the afternoon peak when frequently traffic backs up more than a mile in length. In addition, lengthening one of the left turn lanes on the north approach of SR 121 at the same intersection will improve traffic operations by providing additional storage for the vehicles turning left.

APPENDIX

1. **Turning Movement Counts – January, 2013**
2. **Traffic Signal Timing Sheets**
3. **Collision Summaries**
4. **Synchro/SimTraffic Reports (On CD only)**
5. **Trends Results**
6. **Cost Estimate**

North Central Florida Regional Planning Council

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: SW 8th Avenue Multi-Use Path- 60 Percent Design Plans

JOINT RECOMMENDATIONS

The Citizens Advisory Committee, Bicycle/Pedestrian Advisory Board, Technical Advisory Committee and staff all recommend approval of the the SW 8th Avenue Multi-Use Path 60 Percent Design Plans.

BACKGROUND

As noted in the enclosed Exhibit 1, Alachua County Public Works Department staff has requested that the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area and its advisory committees review the SW 8th Avenue Multi-Use Path 60 Percent Design Plans. Also enclosed are:

Exhibit 2- SW 8th Avenue Multi-Use Path 60% Design Plans slideshow; and

Exhibit 3- SW 8th Avenue Multi-Use Path 60% Construction Plans.

At its meeting on October 1, 2012, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved the "scoping plans" for this project and requested that Alachua County staff bring back 60 percent plans for review.

Enclosures

t:\marlie\ms13\mtpo\memo\sw8avpathjune3.docx

EXHIBIT 1

Marlie Sanderson

From: Brian Singleton [bsingleton@AlachuaCounty.US]
Sent: Wednesday, May 08, 2013 4:34 PM
To: Marlie Sanderson
Cc: Mike Escalante; bateydt@cityofgainesville.org
Subject: June 3 MTPO Meeting Agenda Item

Marlie:

I'm requesting to place the SW 8th Ave Multi-use path project – 60% design plans on the June 3 MTPO meeting agenda and the sub-committee meeting agendas related to the June 3rd meeting. I will send the plans and powerpoint in separate emails since they are large files; if you do not receive either file, let me know.

If you have any questions, please let me know.

Regards,

Brian M. Singleton, E.I.
Transportation Engineering Manager
Alachua County Public Works
5620 NW 120th Lane
Gainesville, FL 32653
352.548.1306 (Desk)
352.260.7830 (Mobile)
352.337.6243 (Fax)
bsingleton@alachuacounty.us

Office Hours: Mon - Thurs, 7:00a to 5:30p

EXHIBIT 2

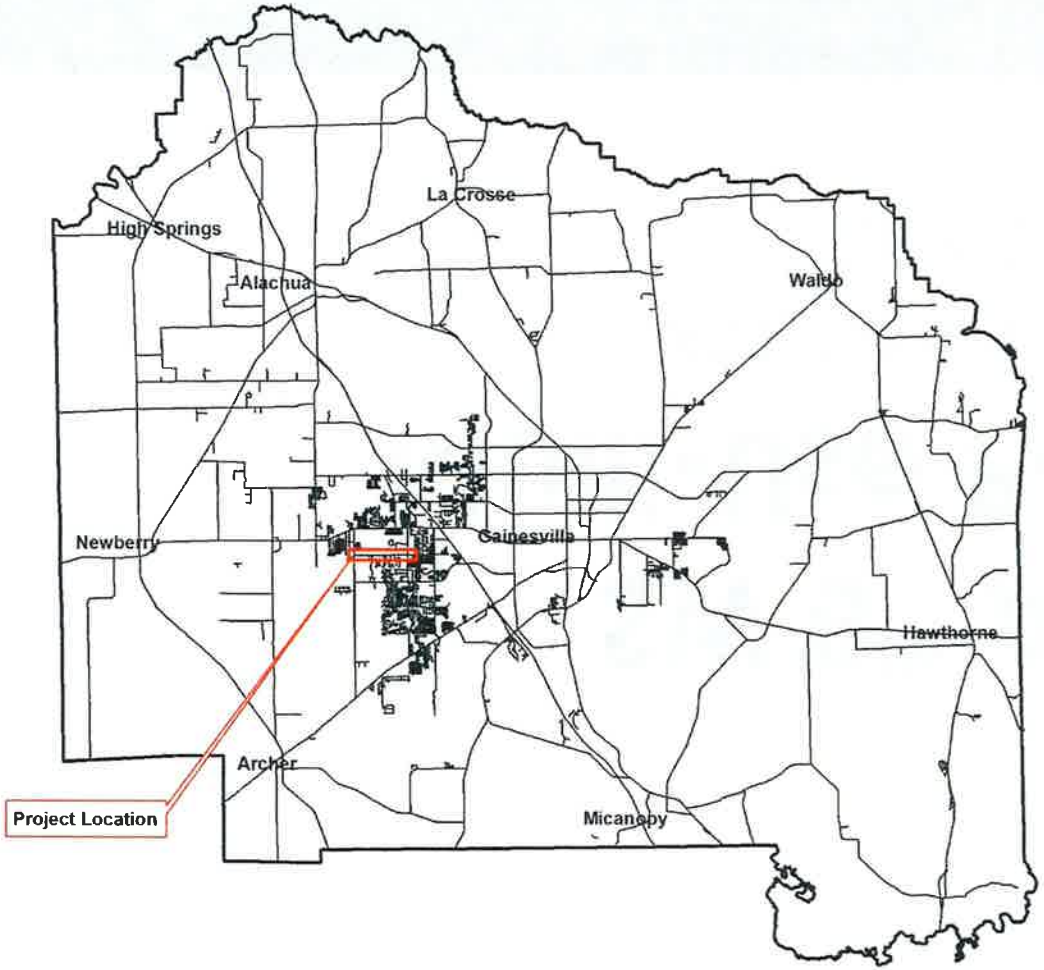
SW 8th Ave Multi-Use Path

60% Design Plans

June 3, 2013

Alachua County, Florida

Project Location



12/10/2009

Project Location

Recommendation

- Approve the 60% design plans
- Direct staff to finalize design and proceed with construction bidding

Presentation Outline

- Project Background
- Review of 60% Design Plans
- Estimated Construction Cost
- Schedule
- Recommendation
- Questions & Comments

Project Background

- #2 Priority of Bike/Ped Work Program
- Scope of work: design & construction of an 8 ft wide multi-use path from SW 91st St to SW 122nd St reducing path width to a minimum of 5 ft in constrained areas
  - Approved by BoCC on September 25, 2012
  - Approved by MTPO on October 1, 2012
- Construction is fully funded through the Federal Transportation Enhancement Program via FDOT

Review 60% Plans

Alachua County, Florida

Existing Conditions – *SW 8th Ave*

- ±2 Miles in Length - SW 122nd St to SW 91st St
- 80' Right-of-Way
- ±30-40' Pavement Width
- Vegetation & Fences abut R/W Line
- Drainage Swales Both Sides. Poorly Defined in Areas
- Driveways & Side Streets
- Utility Poles

INSERT DATE

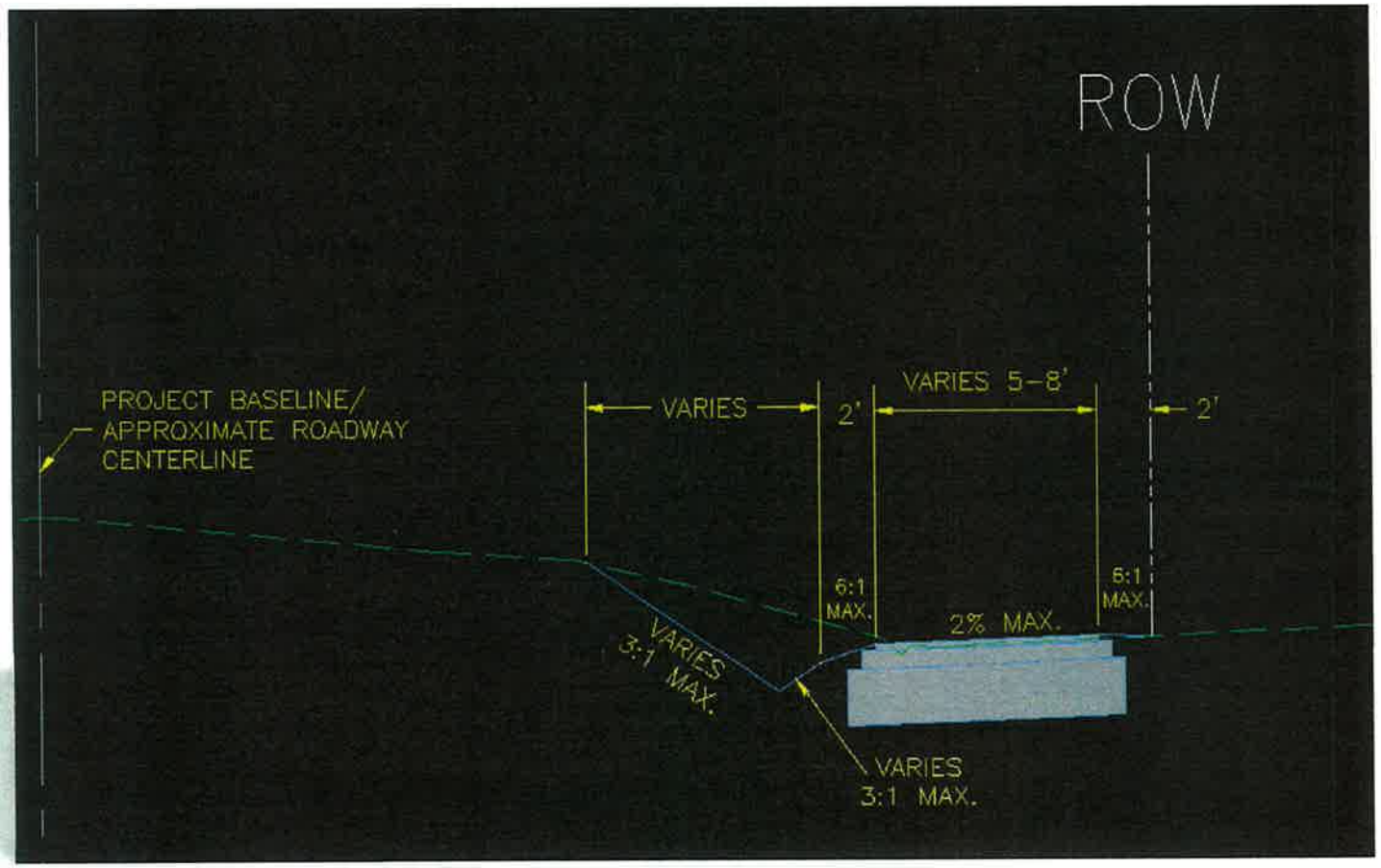
Proposed Conditions – *SW 8th Ave*

- 8' Path Located On South Side of Roadway – 2' Offset from R/W Line
  - Exceptions to 8' Width:
    - Driveway Crossings and Side Drains
    - Areas with Limited Space Due to Center Turn Lanes
    - Runoff Volume Sensitive Drainage Areas
  - Exceptions to 2' Offset:
    - Unmovable Obstacles and Utilities
    - Side-Street Crossings (Visibility at Stop Bar)
- Path Never Less than 6' Wide

Proposed Conditions – *SW 8th Ave*

- Swale Blocks on North Side of Road Between SW 115th St and SW 105th Ter (±0.6 miles)
  - Purpose:
    - Retain Runoff within Volume Sensitive Drainage Area
    - Hayes Glen Subdivision Flooding From 2004
- Compensatory Stormwater Management Facility
  - Purpose:
    - Retain Runoff within Volume Sensitive Drainage Area
    - Royal Oaks Subdivision Flooding From 2004

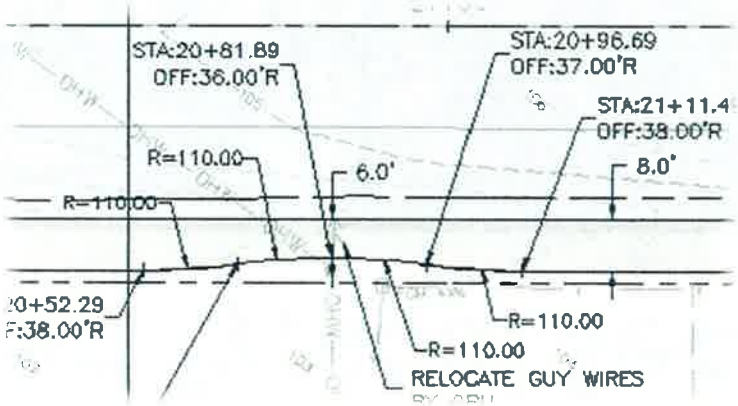
Proposed Typical Section



INSERT DATE

Deviation from 8' Width

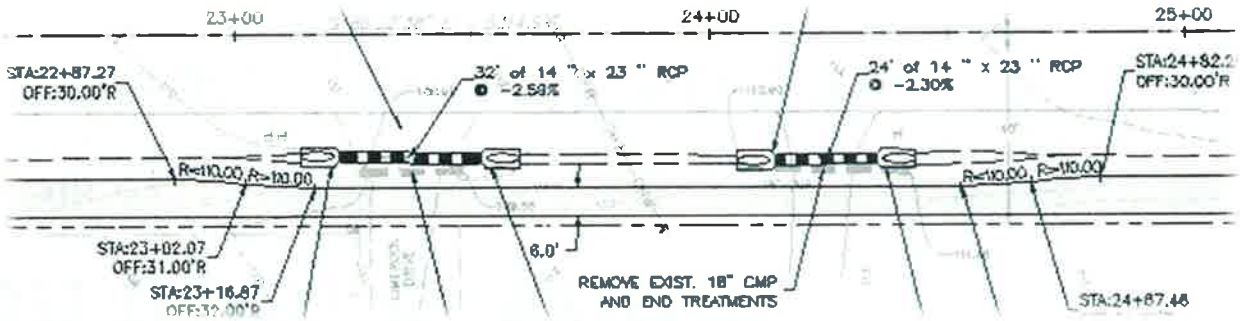
- ±1100 LF East of SW 122nd St
- 6' Wide Path to Avoid Utility Pole
- Spans 60'



INSERT DATE

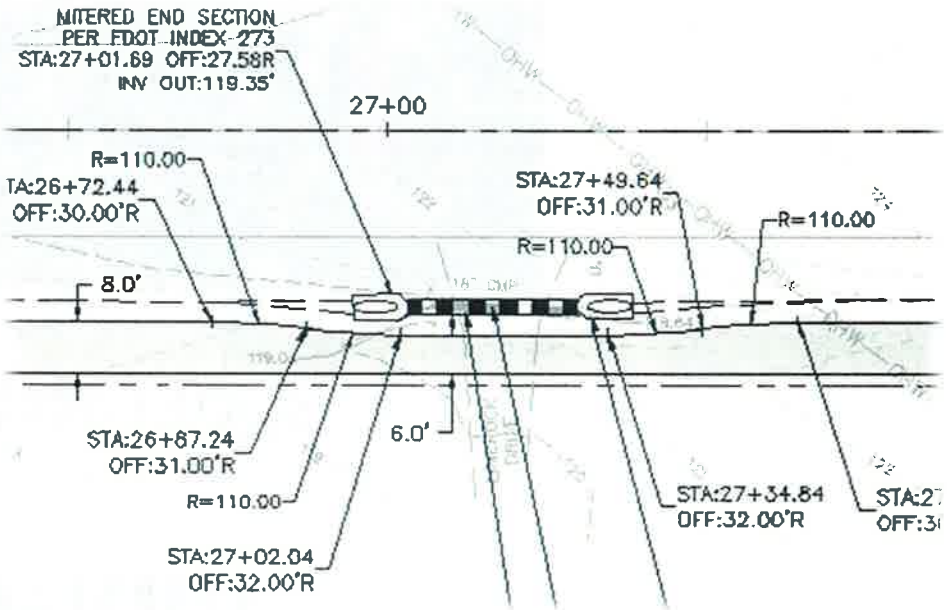
Deviation from 8' Width

- ±1300 LF East of SW 122nd St
- 6' Wide Path Near Driveway Side Drains
- Spans 195'



Deviation from 8' Width

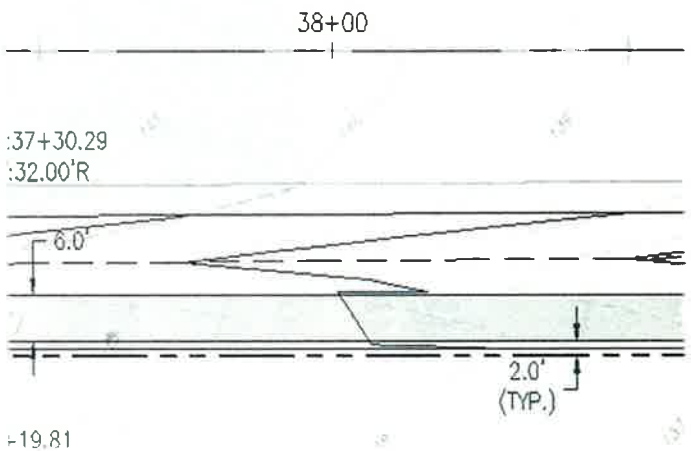
- Near SW 117th St
- 6' Wide Path Near Driveway Side Drain
- Spans 77'



INSERT DATE

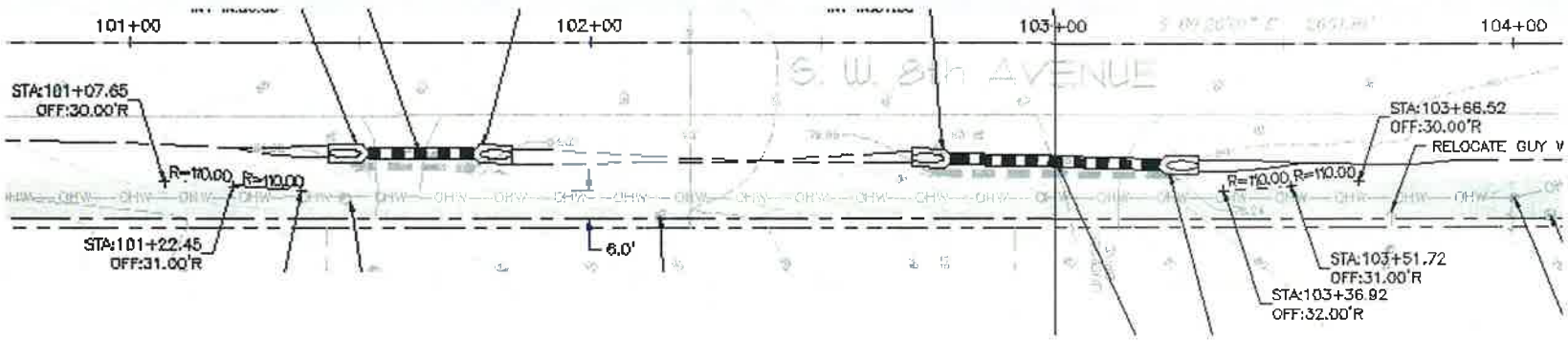
Deviation from 8' Width

- SW 115th St to SW 99th St – Balmoral to Willow Bend Subdivisions
- 6' Wide Path
- Spans 5,375'
- Volume Sensitive Drainage Area - Swale Blocks within Northern Swale to Compensate for Increase in Runoff From Path (between Balmoral and Granite Park)



Deviation from 8' Width

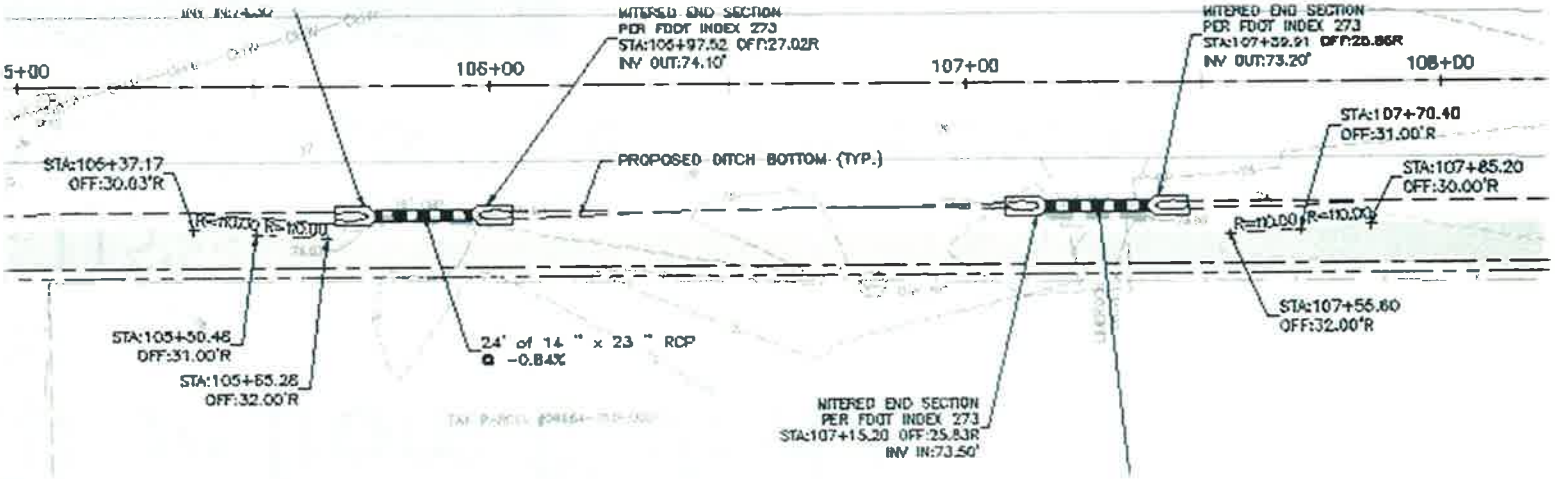
- Between SW 96th St & SW 93th St
- Spans 260'
- 6' Wide Path Near Driveway Side Drains



INSERT DATE

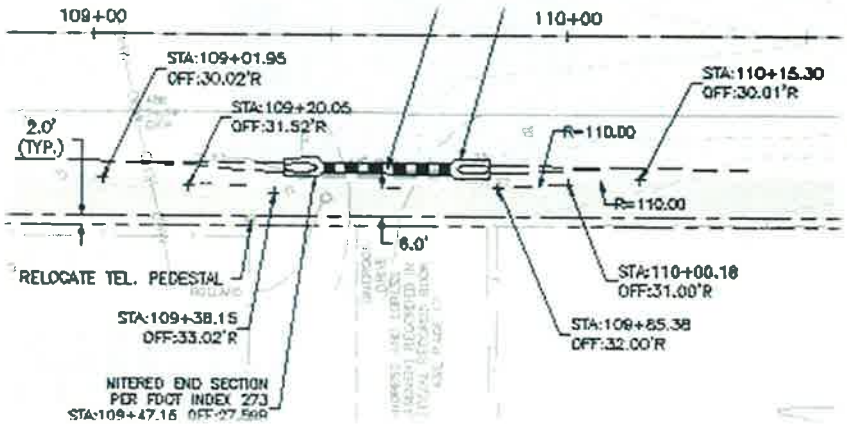
Deviation from 8' Width

- Between SW 96th St & SW 93rd St
- Spans 250'
- 6' Wide Path Near Driveway Side Drains



Deviation from 8' Width

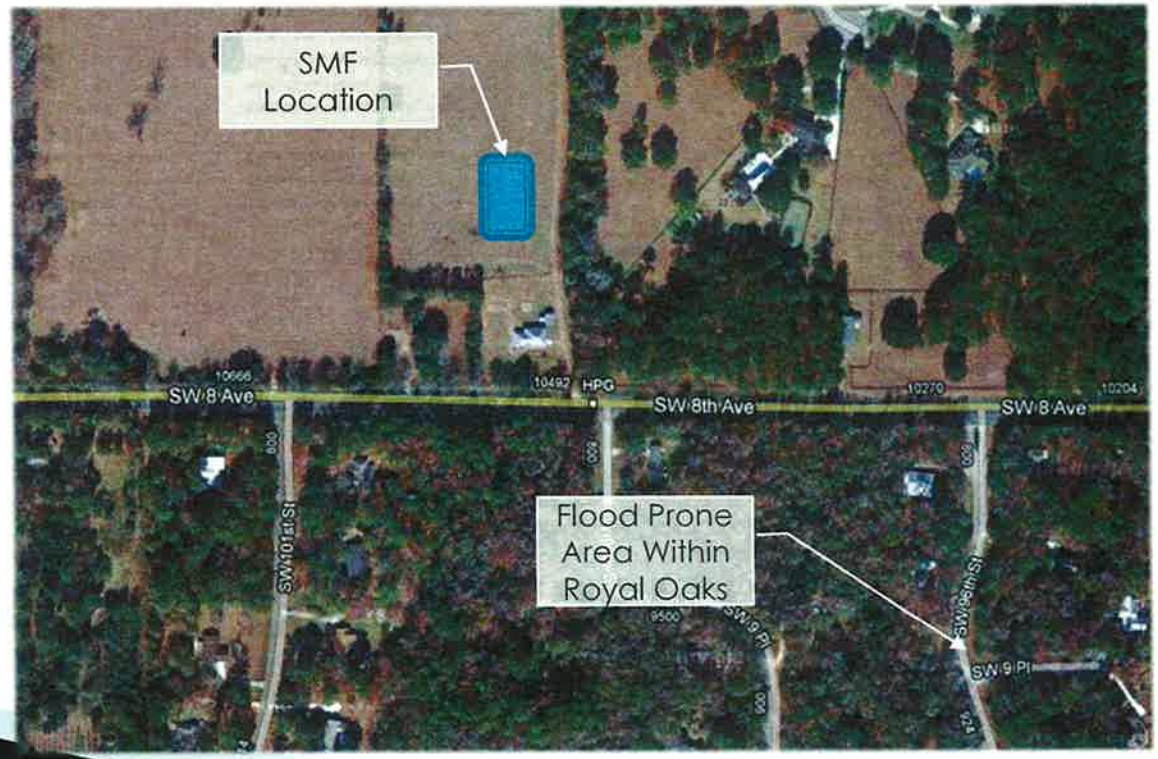
- SW 93rd St
- Spans 115'
- 6' Wide Path Near Driveway Side Drain



INSERT DATE

Compensatory SMF

- Across From SW 99th St
- Volume Sensitive Drainage Area
- Stores Runoff Upgradient of Flood Prone Area Within Royal Oaks Subdivision



INSERT DATE

Construction Cost Estimate

- $780,000±

SW 8th Ave Multi-Use Path Preliminary Opinion of Probable Cost

60% Plans

FDOT Pay Item			Item	QTY	Unit	Unit Price	Amount
104	10	3	Sediment Barrier	9,768	LF	$0.68	$6,642.24
0000	200	1	Prevention, Control, and Abatement of Erosion and Water Pollution	1	LS	$15,000.00	$15,000.00
120	1		Excavation	2281	CY	$3.50	$7,983.11
120	4		Subsoil Excavation	800	CY	$7.21	$5,768.00
120	6		Embankment	2472	CY	$4.00	$9,888.74
160	4		12" LBR40 Stabilization	9,762	SY	$2.79	$27,235.98
285	70	1	4" Limerock (Optional Base Group 1)	8,647	SY	$9.15	$79,120.05
337	7	30	1.5" SP-9.5 Asphalt Concrete	621	TN	$99.13	$61,578.57
400	1	2	Concrete Class 1, Endwalls	9.97	CY	$858.66	$8,560.84
400	1	11	Conc. Retaining Wall	73	CY	$712.11	$51,855.85
425	2	71	Manholes, J-7, <10'	4	EA	$6,364.38	$25,457.52
425	15	21	Inlets, Ditch Bottom, Type C, <10'	5	EA	$1,455.91	$7,279.55
425	19	10	Inlets, Closed Flume	1	EA	$2,780.60	$2,780.60
430	174	124	Pipe Culvert, Opt Mtl, Round, 24"	7	LF	$75.00	$525.00
430	175	218	Pipe Culvert, Opt Mtl, Elliptical, 18"	924	LF	$45.00	$41,580.00
430	175	224	Pipe Culvert, Opt Mtl, Elliptical, 24"	330	LF	$50.00	$16,500.00
430	175	236	Pipe Culvert, Opt Mtl, Elliptical, 36"	8	LF	$100.00	$800.00
430	982	625	Mitered End Section, Opt Elliptical 18"	26	EA	$841.45	$21,877.70
430	982	629	Mitered End Section, Opt Elliptical 24"	1	EA	$870.00	$870.00
515	1	2	Pipe Handrail - Guiderail, Aluminum	662	LF	$42.58	$28,187.96
522	2		Sidewalk Concrete, 6" Thick	30	SY	$50.00	$1,500.00
524	1	2	Concrete Ditch Pavement, Non Reinf, 4" (Pond Spillway)	90	SY	$60.00	$5,400.00
570	1	2	Performance Turf, SOD	15488	SY	$1.96	$30,356.48
						Subtotal	$456,748.19
			Mobilization, MOT, Clearing, Grubbing, Striping, Signage, Etc.			35%	$159,861.87
						Subtotal	$616,610.05
			CEI			10%	$61,661.01
						Subtotal	$678,271.06
			Contingency			15%	$101,740.66
						Total	$780,011.72

INSERT DATE

Schedule

- Present 60% design plans to MTPO June 3, 2013
- Finalize design November 2013
- Construction Fiscal Year 2014

Recommendation

- Approve the 60% design plans
- Direct staff to finalize design and proceed with construction bidding

Questions/Comments

SW 8th Ave Multi-use Path

Alachua County, Florida

EXHIBIT 3

60% CONSTRUCTION PLANS
for
SW 8TH AVENUE MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

Alachua County, Florida

COUNTY COMMISSIONERS:

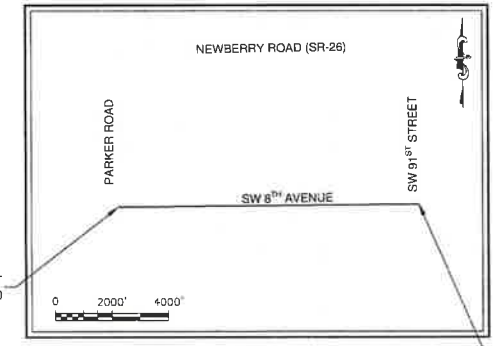
MIKE BYERLY, CHAIR

LEE PINKOSON, VICE CHAIR

SUSAN BAIRD

ROBERT HUTCHINSON

CHARLES S. CHESTNUT



LOCATION MAP

1" = 2000' (22 x 34)
1" = 4000' (11 x 17)

INDEX OF DRAWINGS

GOVERNING STANDARDS & SPECIFICATIONS
FLORIDA DEPARTMENT OF TRANSPORTATION, DESIGN STANDARDS DATED 2012, AND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION DATED 2010, AS AMENDED BY CONTRACT DOCUMENTS

DESIGN CRITERIA
FDOT "MANUAL OF UNIFORM MINIMUM STANDARDS FOR DESIGN, CONSTRUCTION AND MAINTENANCE FOR STREETS AND HIGHWAYS" MAY 2007 EDITION

MAY, 2013

DRMP PROJECT NO. 11-0160.002

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ROADWAY PLANS
ENGINEER OF RECORD: CHRISTOPHER D. TOWNE, P.E.

FOR REVIEW ONLY
NOT FOR CONSTRUCTION

NO	DATE	REVISIONS	BY

DESIGNED BY JPK
DRAWN BY JPK
CHECKED BY CDT
APPROVED BY CDT

COVER SHEET

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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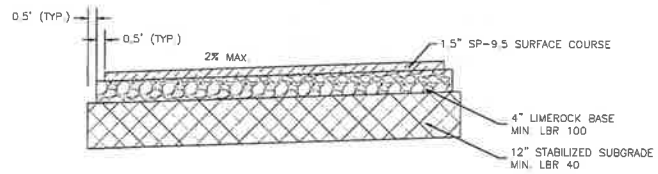
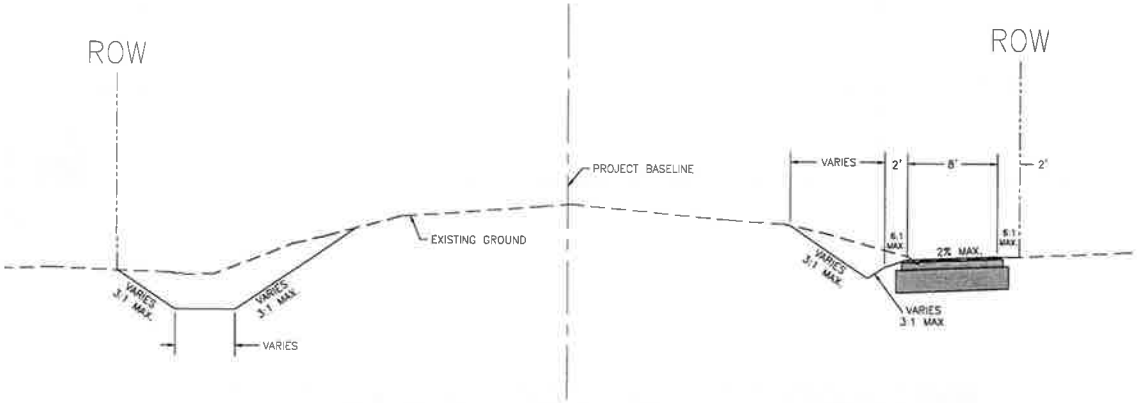
CHRISTOPHER D. TOWNE, P.E.
FLORIDA P.E. NO. 68608

PROJECT NO. 11-0160.002
SCALE NONE
DATE MAY 2013
DRAWING 1



TYPICAL PAVEMENT SECTION



TYPICAL SECTION

REVISIONS		
NO.	DATE	DESCRIPTION

DESIGNED BY: JFW
DRAWN BY: JFW
CHECKED BY: CDT
APPROVED BY: CDT

TYPICAL SECTIONS

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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NOT FOR CONSTRUCTION

CHRISTOPHER D. TOWNE, P.E.
FLORIDA P.E. NO.: 66628

PROJECT NO. 11-0180 002
SCALE: AS SHOWN
DATE: MAY 2013
DRAWING: 6

GENERAL NOTES:

THIS DESIGN HAS BEEN BASED UPON TOPOGRAPHICAL FIELD SURVEY BY ALACHUA COUNTY PUBLIC WORKS

CONTRACTOR SHALL FAMILIARIZE HIMSELF WITH THE SITE, INCLUDING ALL SURFACE AND SUB-SURFACE CONDITIONS, THE WORK REQUIRED AND ALL OTHER CONDITIONS THAT MAY EFFECT THE SUCCESSFUL COMPLETION OF THE JOB PRIOR TO COMMENCEMENT OF WORK.

THE CONTRACTOR SHALL GIVE ALL NOTICES AND COMPLY WITH ALL LAWS, ORDINANCES, RULES, REGULATIONS AND PERMIT CONDITIONS BEARING ON THE CONDUCT OF THE WORK, AS DRAWN AND SPECIFIED. IF THE CONTRACTOR OBSERVES THAT THE DRAWINGS AND SPECIFICATIONS ARE AT VARIANCE THEREWITH, HE SHALL PROMPTLY NOTIFY THE ENGINEER, IN WRITING, AND ANY NECESSARY CHANGES SHALL BE ADJUSTED, AS PROVIDED IN THE AGREEMENT FOR CHANGES IN THE WORK.

THE CONTRACTOR SHALL BE RESPONSIBLE TO THE OWNER AND THE ENGINEER FOR THE ACTS AND OMISSIONS OF CONTRACTOR'S EMPLOYEES AND ALL HIS SUBCONTRACTORS AND THEIR AGENTS AND EMPLOYEES AND OTHER PERSONS PERFORMING ANY OF THE WORK UNDER A CONTRACT WITH THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAKING ALL NECESSARY ARRANGEMENTS WITH GOVERNMENTAL DEPARTMENTS, PUBLIC UTILITIES, PUBLIC CARRIERS, SERVICE COMPANIES, AND CORPORATIONS OWNING OR CONTROLLING ROADWAYS, RAILWAYS, WATER, SEWER, GAS, ELECTRICAL, TELEPHONE, AND TELEGRAPH FACILITIES SUCH AS PAVEMENTS, TRACKS, PIPING, WIRES, CABLES, CONDUITS, POLES, GUYS, OR OTHER SIMILAR FACILITIES, INCLUDING INCIDENTAL STRUCTURES CONNECTED THEREWITH THAT ARE ENCOUNTERED IN THE WORK IN ORDER THAT SUCH ITEMS MAY BE PROPERLY SUPPORTED, PROTECTED OR LOCATED.

UNLESS OTHERWISE SPECIFIED IN THE GENERAL CONDITIONS, ALL CONSTRUCTION IS TO BE GOVERNED BY THE PLANS, APPLICABLE PERMITS, AND SPECIFICATIONS HEREIN, AND ALL APPLICABLE FEDERAL, STATE AND LOCAL BUILDING AND SAFETY CODES, LAWS AND ORDINANCES.

PRIOR TO PERFORMING ANY WORK WITHIN ANY PUBLIC OR UTILITY RIGHT-OF-WAY, CONTRACTOR SHALL OBTAIN AUTHORIZATION AND PERMIT FROM JURISDICTION RESPONSIBLE FOR SUCH RIGHT-OF-WAY. IN ADDITION, CONTRACTOR SHALL CONTACT SUNSHINE ONE CALL (811) AT LEAST 72 HOURS PRIOR TO START OF WORK.

PRIOR TO PERFORMING ANY WORK WITHIN ANY PUBLIC RIGHT-OF-WAY, CONTRACTOR SHALL DEVELOP AND IMPLEMENT A TRAFFIC CONTROL PLAN CONSISTENT WITH THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" PUBLISHED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION.

IN THE EVENT THE CONTRACTOR DISCOVERS ANY ERRORS OR OMISSIONS IN THE PLANS HE SHALL IMMEDIATELY NOTIFY THE OWNER OR OWNER'S AGENT.

CONTRACTOR SHALL PRESERVE AND PROTECT ALL PERMANENT REFERENCE MONUMENTS, PERMANENT CONTROL POINTS, PERMANENT BENCH MARKS AND PROPERTY CORNERS. IN THE EVENT THE MONUMENTS, POINTS OR MARKERS ARE DISTURBED THE CONTRACTOR SHALL EMPLOY A FLORIDA REGISTERED LAND SURVEYOR TO RESET OR REPLACE THEM.

THE OWNER, OWNER'S AGENT AND INSPECTORS OF APPLICABLE GOVERNMENT JURISDICTIONS, SHALL AT ALL TIMES HAVE ACCESS TO THE WORK WHEREVER AND WHENEVER IT IS IN PREPARATION OR PROGRESS; AND THE CONTRACTOR SHALL PROVIDE PROPER FACILITIES FOR SUCH ACCESS AND FOR THE INSPECTION.

IT IS THE CONTRACTOR'S RESPONSIBILITY TO TAKE ALL REASONABLE AND PRUDENT PRECAUTIONS TO INSURE THAT ALL COMPLETED WORK, MATERIALS AND EQUIPMENT STORED ON SITE ARE SAFE AND SECURED FROM UNAUTHORIZED ACCESS OR USE. SUCH PRECAUTIONS MAY INCLUDE INSTALLATION OF SIGNS, FENCES, OR POSTING OF SECURITY GUARDS.

CONTRACTOR SHALL, AT ALL TIMES, UTILIZE ALL NORMALLY ACCEPTED AND REASONABLY EXPECTED SAFETY PRACTICES AND COMPLY WITH ALL FEDERAL, STATE AND LOCAL REGULATIONS, ORDINANCES AND GUIDELINES PERTAINING TO SAFE UTILIZATION OF EQUIPMENT OR MATERIALS AS PUBLISHED BY MANUFACTURER.

PRIOR TO INITIATING ANY EXCAVATION (INCLUDING BUT NOT LIMITED TO TUNNELS, DITCHES, STORMWATER PONDS, CANALS, ARTIFICIAL LAKES) CONTRACTOR SHALL INSTALL FENCES AND TAKE ALL OTHER REASONABLE AND PRUDENT STEPS TO INSURE THAT ACCESS TO EXCAVATION BY UNAUTHORIZED PERSONNEL IS PREVENTED.

CONTRACTOR SHALL COMPLY IN EVERY RESPECT WITH THE PROVISIONS OF THE FLORIDA STATE TRENCH SAFETY ACT.

ADEQUATE TRAFFIC CONTROL, BARRICADES AND FLAGMAN SERVICES SHALL BE FURNISHED AND MAINTAINED BY THE CONTRACTOR AT ALL POINTS WHERE CONVEYING EQUIPMENT ENGAGED ON THE WORK REGULARLY ENTERS ONTO OR CROSSES TRAFFIC-CARRYING ROADS.

THE CONTRACTOR SHALL COMPLY IN EVERY RESPECT WITH THE FEDERAL OCCUPATIONAL HEALTH AND SAFETY ACT OF 1970 AND ALL RULES AND REGULATIONS NOW OR HEREAFTER IN EFFECT UNDER SAID ACT, AND THE CONTRACTOR FURTHER AGREES TO COMPLY WITH ANY AND ALL APPLICABLE STATE LAWS AND REGULATIONS PERTAINING TO JOB SAFETY AND HEALTH.

THE CONTRACTOR SHALL DEVELOP AND IMPLEMENT AN EROSION CONTROL PLAN TO MINIMIZE EROSION AND INSURE FUNCTIONING OF STORMWATER MANAGEMENT SYSTEM UPON COMPLETION OF CONSTRUCTION.

GENERAL NOTES (CONT):

CONTRACTOR FURTHER AGREES THAT CONTRACTOR AND ITS SUBCONTRACTORS SHALL NOT CAUSE THE DISCHARGE, RELEASE OR DISPOSAL OF ANY HAZARDOUS MATERIAL CREATED BY ITS WORK ON OR ABOUT THE JOB SITE. IN THE EVENT OF ANY SPILL, RELEASE OR ANY OTHER REPORTABLE OCCURRENCE, CONTRACTOR SHALL NOTIFY THE APPROPRIATE GOVERNMENTAL AGENCY AND SHALL TAKE SUCH ACTION AS MAY BE NECESSARY TO MINIMIZE THE DELETERIOUS EFFECT OF SUCH SPILL ON PERSONS OR PROPERTY.

THE EXISTING UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL FIELD LOCATE ALL EXISTING UTILITIES AS TO SIZE, LOCATION, AND ELEVATION. THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ANY AND ALL CONFLICTS PRIOR TO BEGINNING CONSTRUCTION.

STABILIZED SUBGRADE MAY NEGLECTED IF DIRECTED IN THE FIELD BY THE OWNER PROVIDED THAT COMPACTION REQUIREMENTS FOR THE LIMEROCK BASE ARE ABLE TO BE MET WITH THE IN-SITU MATERIAL.

SPREAD FOOTING REQUIREMETS FOR CAST-IN-PLACE GRAVITY WALL:

SUBGRADE BENEATH FOUNDATIONS SHALL BE COMPACTED TO 95% OF AASHTO T-180 FOR A DEPTH OF 1 FOOT FOR SOIL STRATUMS 1 AND 2. STRATUM 3 SOILS SHALL BE PROBED TO CONFIRM THEY ARE FIRM AND UNYIELDING IN LIEU OF PERFORMING COMPACTION TESTS AS LONG AS THESE SOILS ARE NOT APPRECIABLY DISTURBED. DISTURBED STRATUM 3 SOILS IF STRATUM 4 SOILS ARE ENCOUNTERED AT THE FOUNDATION BOTTOM ELEVATION, THESE SOILS SHOULD BE UNDERCUT 1 FOOT AND REPLACED WITH STRATUM 2 OR 3 SOILS THAT ARE COMPACTED TO 95%.

LEGEND OF SYMBOLS & ABBREVIATIONS:

- 2 1 / 1 12 — SECTION CORNER SHOWING SECTIONS
- CONCRETE SURFACE
- ASPHALT SURFACE
- BENCHMARK LOCATION
- STA: 11+00 or 11+00 — STATIONING DESIGNATION
- 1 — PLATTED LOT NUMBER
- RIGHT OF WAY LINES
- TAX PARCEL LINES
- 100 — CONTOUR LINE (WITH ELEVATION)
- CENTERLINE OF RIGHT OF WAY
- OHW — OHW — OHW — OVERHEAD UTILITY WIRES
- x — FENCE LINE
- 48" CMP — STORM PIPE (WITH SIZE, TYPE OF MATERIAL, AND INVERT ELEVATION)
- INV — INVERT
- CMP — CORRUGATED METAL PIPE
- RCP — REINFORCED CONCRETE PIPE
- FCM — FOUND CONCRETE MONUMENT
- FIR — FOUND IRON ROD
- I.D. — IDENTIFICATION
- S.C. — SWEET GUM
- WOODEN UTILITY POLE
- GUY ANCHOR
- CONCRETE UTILITY POLE
- PIPE "E" INLET
- MAILBOX
- CLEAN OUT
- WATER VALVE
- FIRE HYDRANT
- TELEPHONE PEDESTAL
- UTILITY HAND HOLE
- CABLE BOX
- GAS VALVE
- MITERED END SECTION
- STORM MANHOLE
- SANITARY SEWER MANHOLE
- TRANSFORMER
- ELECTRIC MANHOLE
- TELEVISION BOX

REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY	JPW
DRAWN BY	JPW
CHECKED BY	CDI
APPROVED BY	CDI

GENERAL NOTES, LEGEND AND ABBREVIATIONS

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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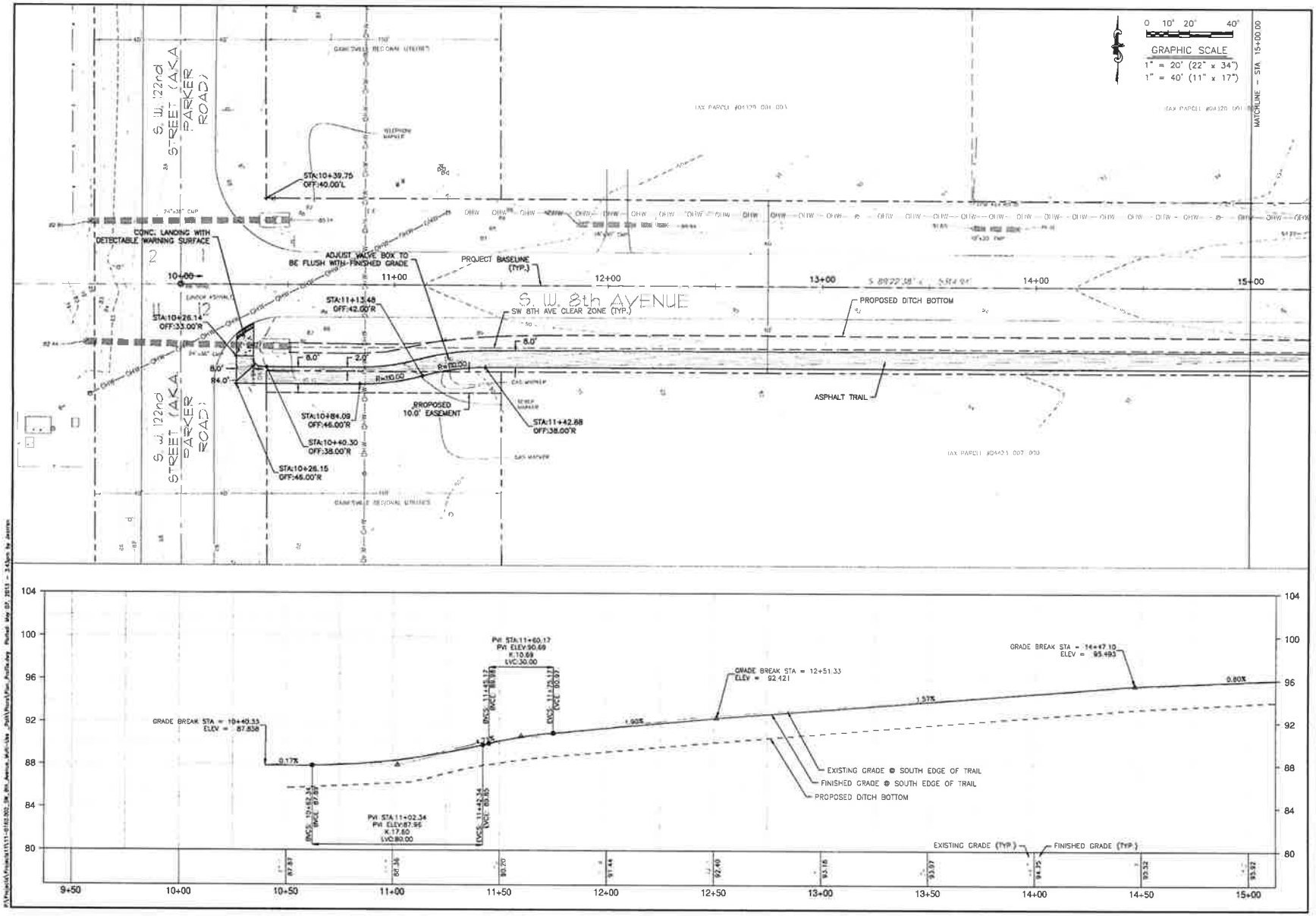

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DRAWN BY JTW
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APPROVED BY CDT

PLAN & PROFILE
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SITE ENGINEERING PLANS FOR
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FLORIDA P.E. NO.: 59828

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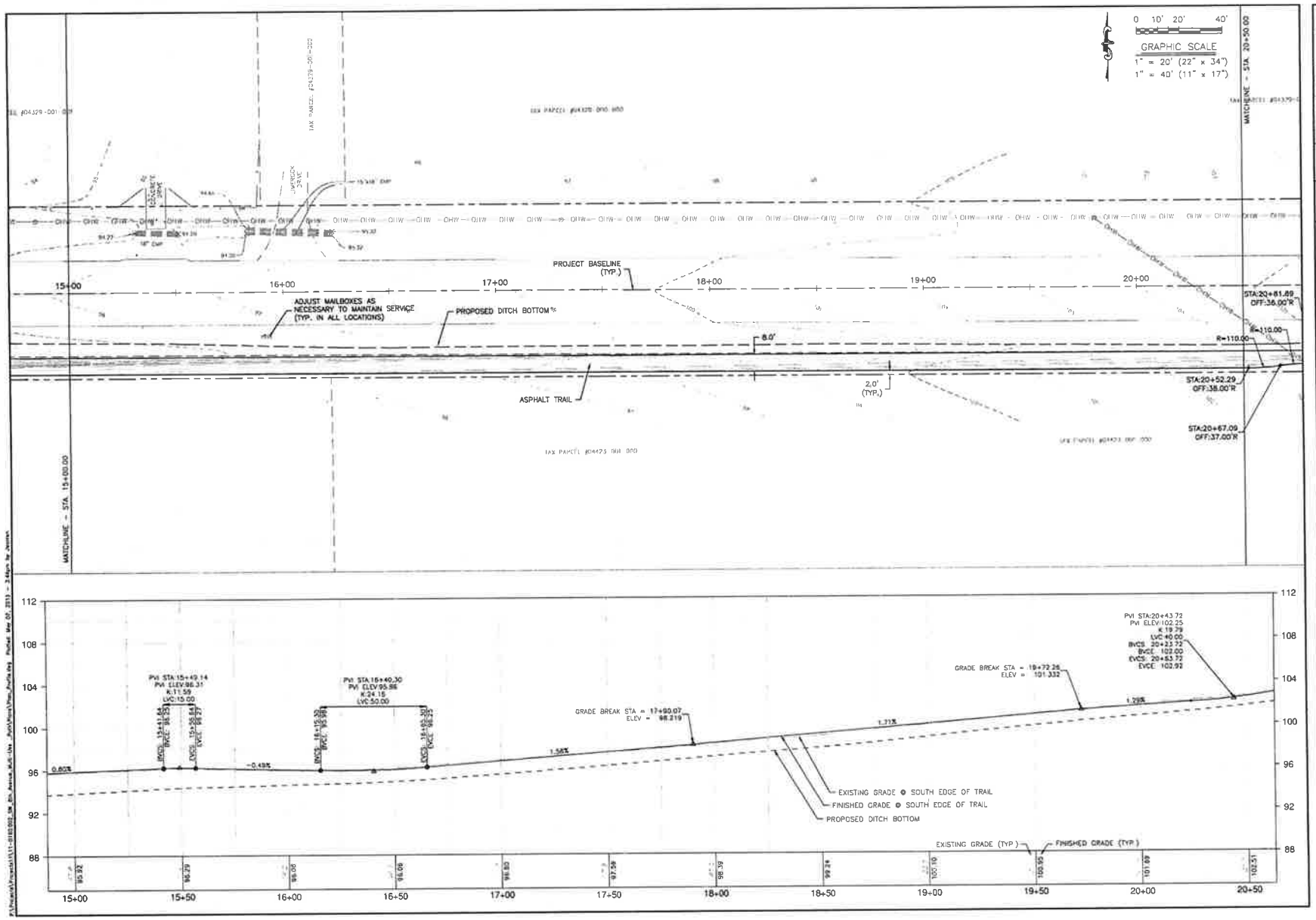

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PLAN & PROFILE
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SITE ENGINEERING PLANS FOR
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"AS-BUILT" PLANS BASED ONLY ON FIELD CONSTRUCTION

CHRISTOPHER S. SOWNE, P.E.
FLORIDA P.E. NO. 64828

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SCALE 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE MAY 2013
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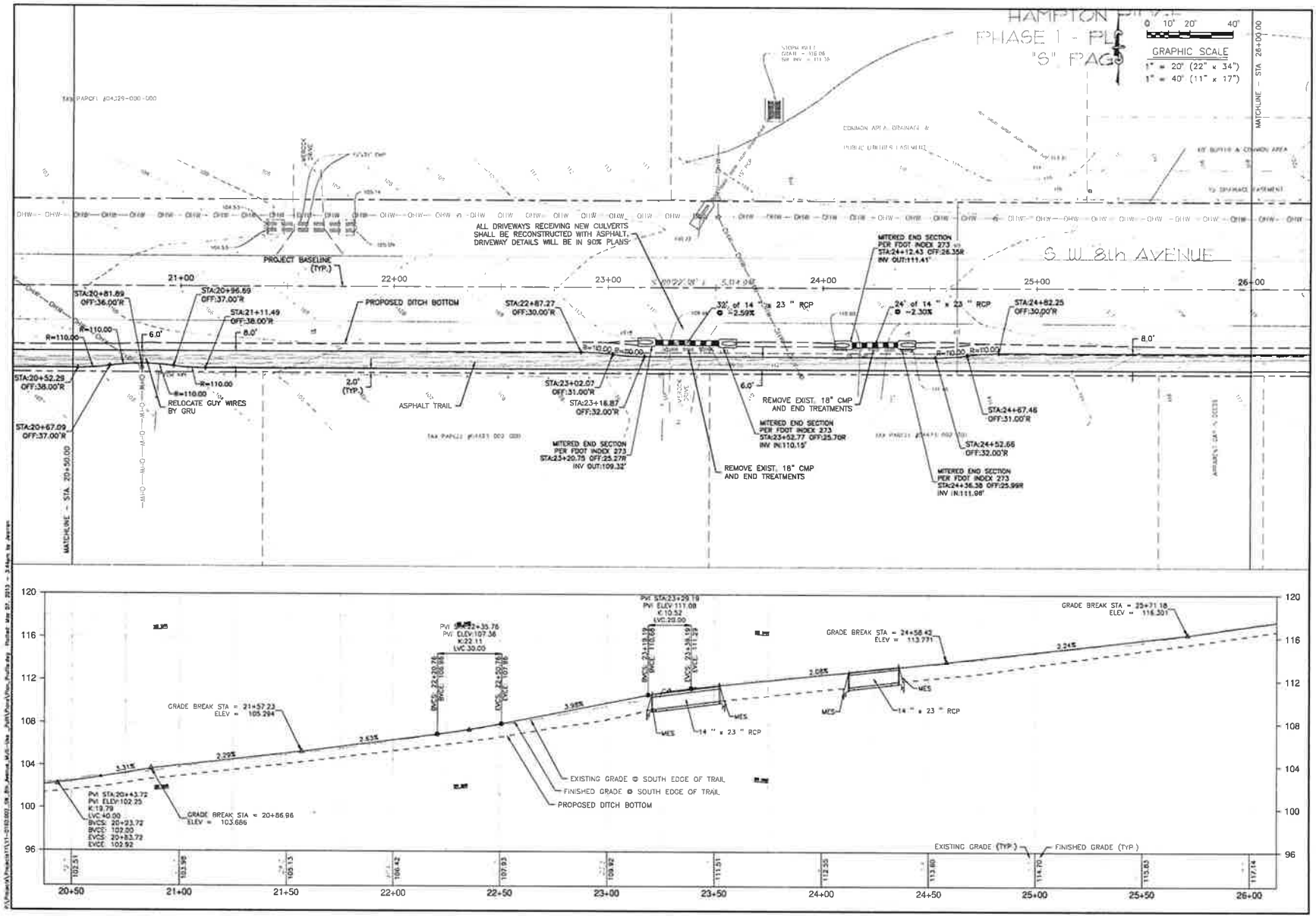

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

PLAN & PROFILE
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FLORIDA P.E. NO. 66618

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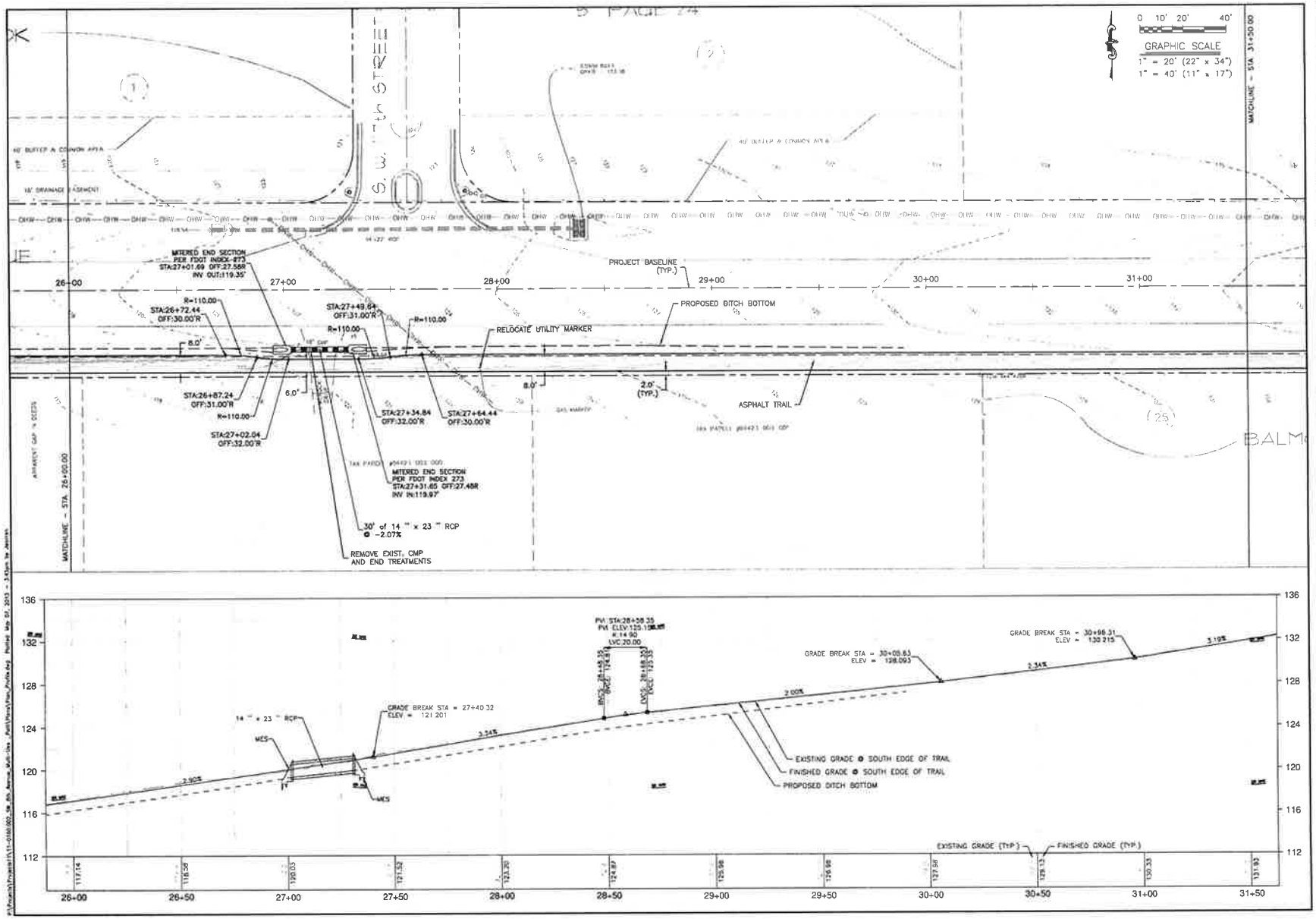

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PROJECT NO: 11-0160 002
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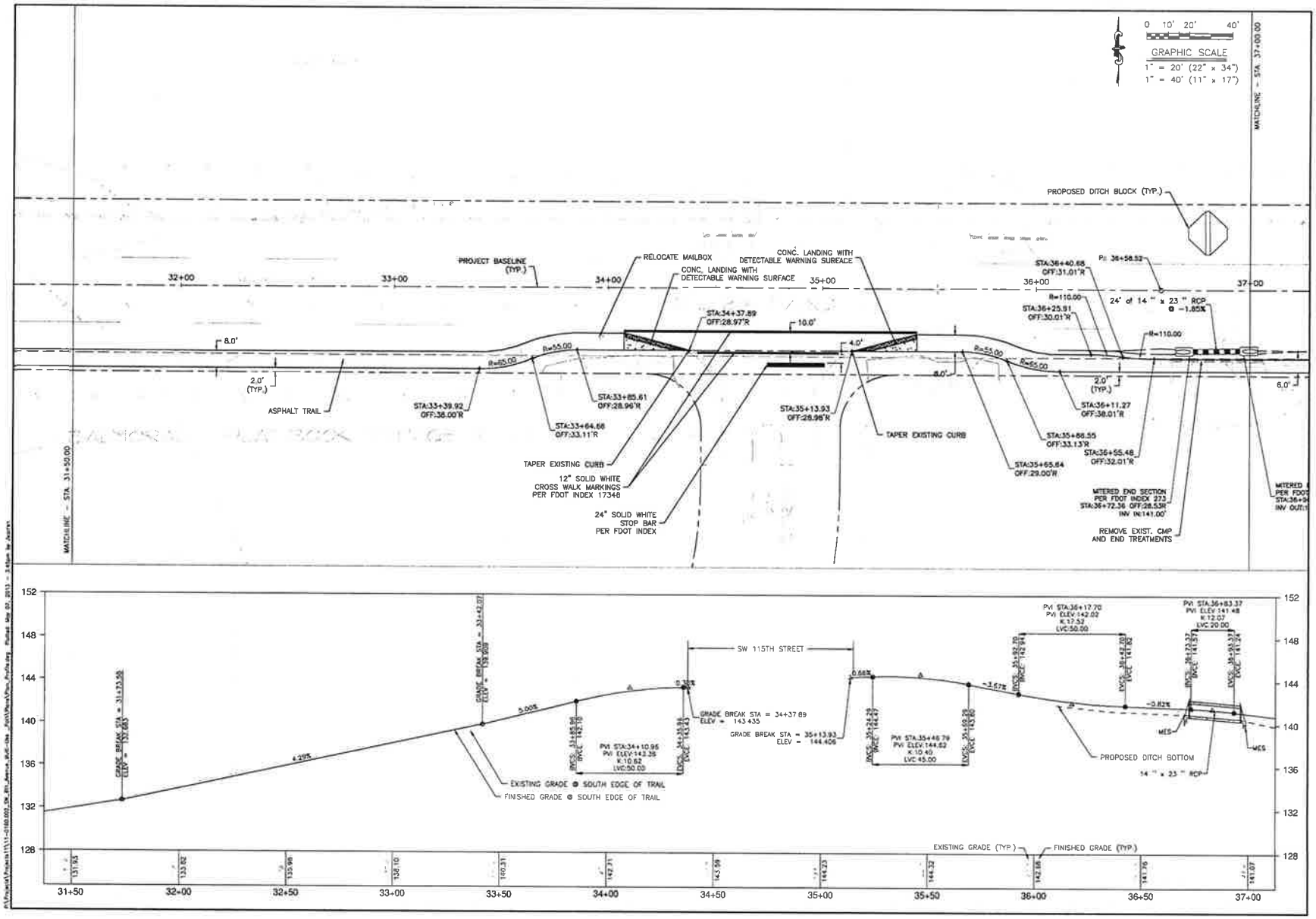
DRAWING: 11



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APPROVED BY CDT

PLAN & PROFILE
STA. 31+50 TO STA. 37+00

SITE ENGINEERING PLANS FOR
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MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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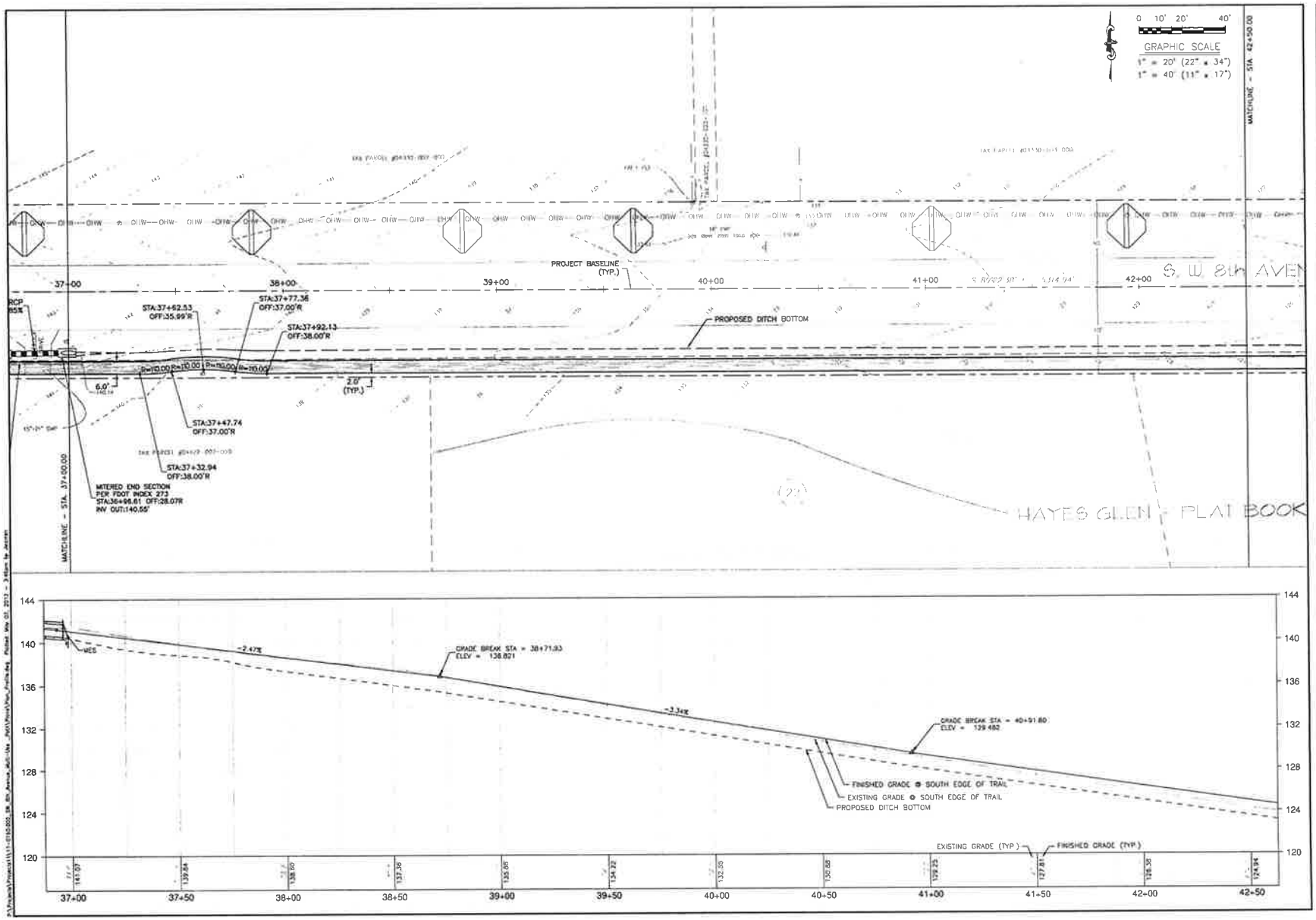

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APPROVED BY CDI

PLAN & PROFILE
STA. 37+00 TO STA. 42+50

SITE ENGINEERING PLANS FOR:
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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CHRISTOPHER S. CRAWL, P.E.
FLORIDA P.E. NO. 69928

PROJECT NO. 11-0160.002
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DATE MAY 2013
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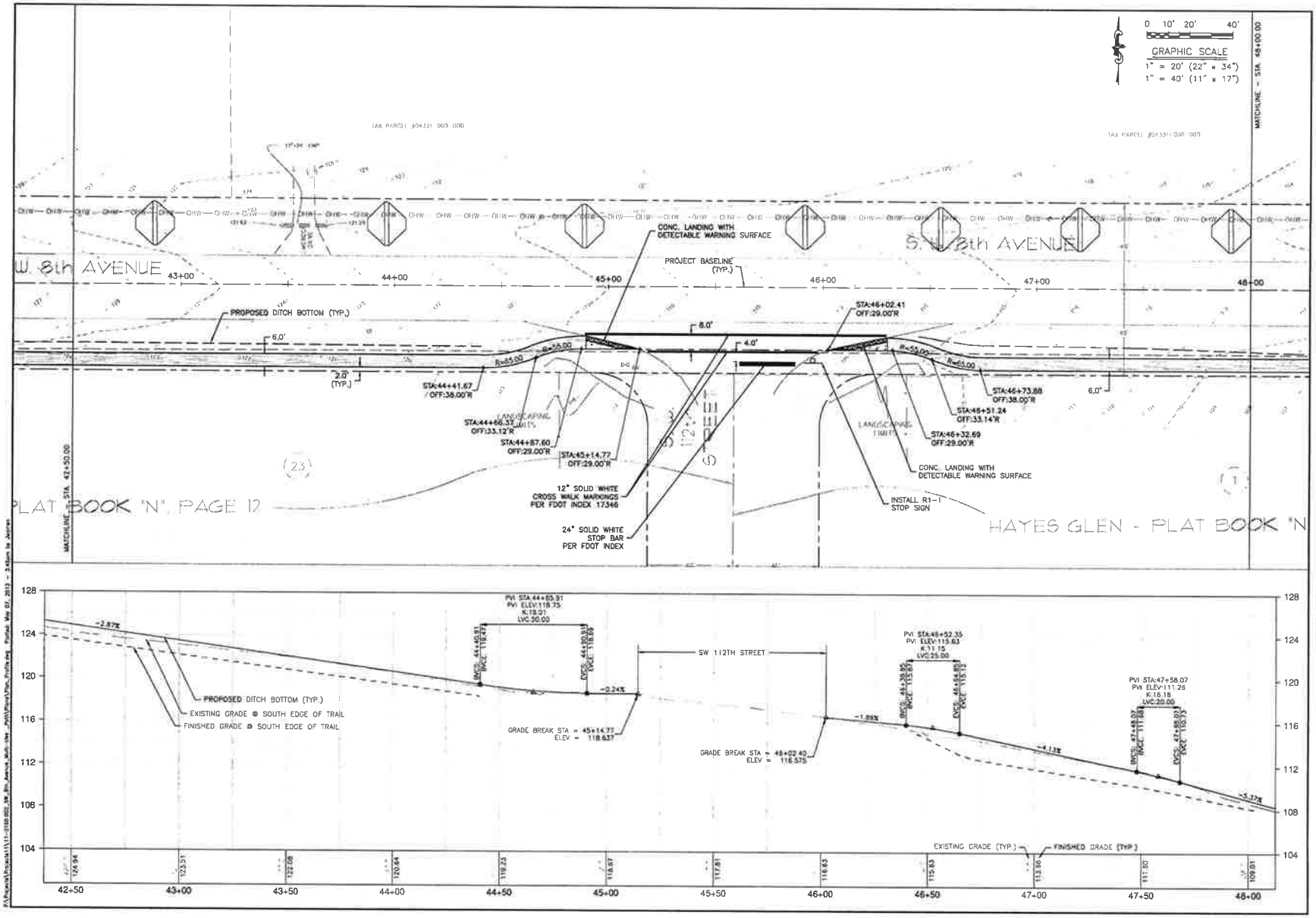

REVISIONS			
NO	DATE	DESCRIPTION	BY

DESIGNED BY: JTW
DRAWN BY: JTW
ENGINEER: CDT
APPROVED BY: CDT

PLAN & PROFILE
STA. 42+50 TO STA. 48+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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CHRISTOPHER S. TOWNE, P.E.
FLORIDA P.E. NO. 69528

PROJECT NO: 11-0160.002
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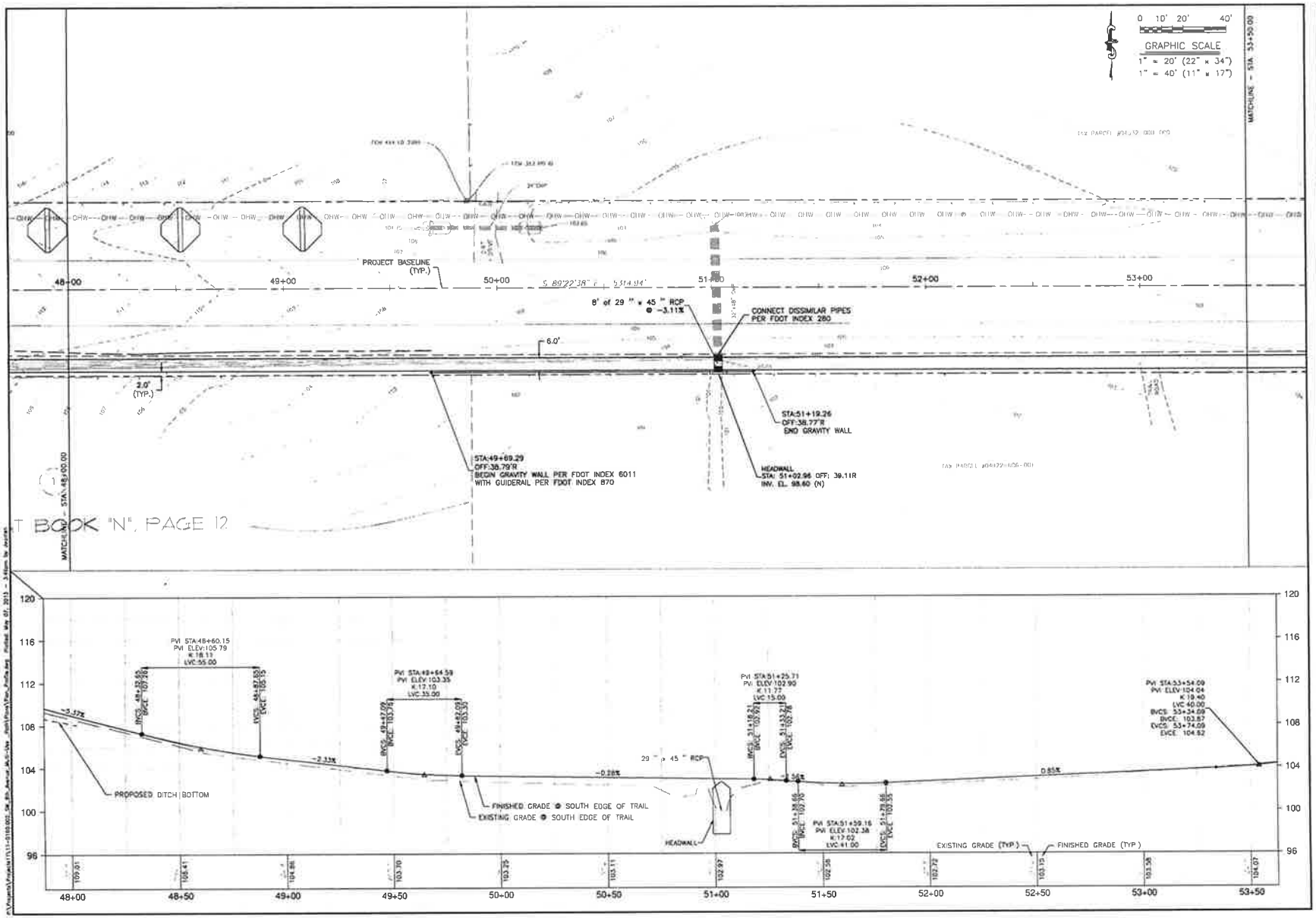
DATE: MAY 2013
DRAWING: 14



SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

PLAN & PROFILE
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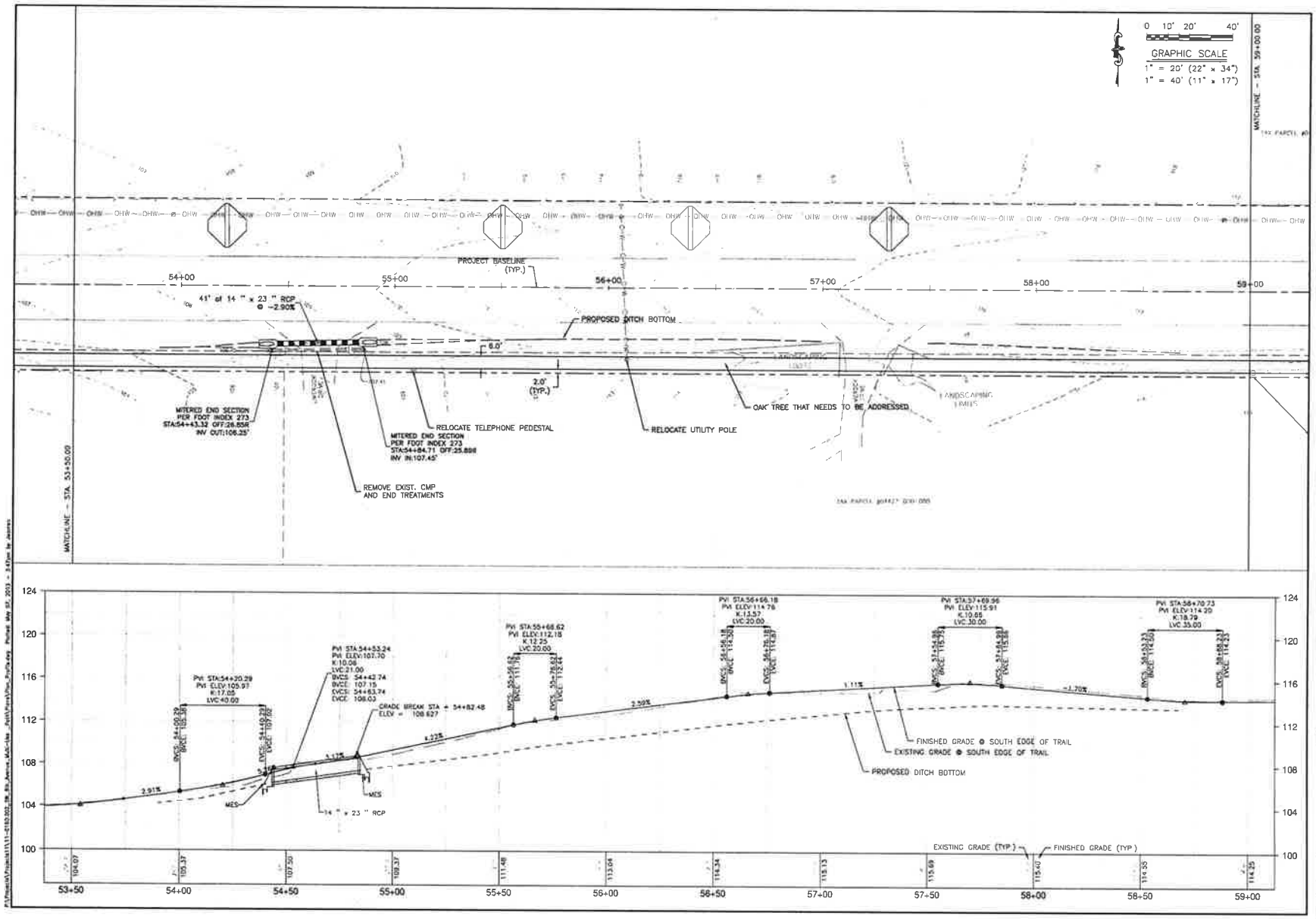
DESIGNED BY	DRAWN BY	CHECKED BY	APPROVED BY
JPW	JPW	CDT	CDT

REVISIONS

NO.	DATE	DESCRIPTION	BY

CHRISTOPHER D. TOWNE, P.E.
FLORIDA P.E. NO. 58408

PROJECT NO: 11-0160 002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING: 15



REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY JTW
DRAWN BY JTW
CHECKED BY CDT
APPROVED BY CBT

PLAN & PROFILE
STA. 53+00 TO STA. 59+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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54-5-MP
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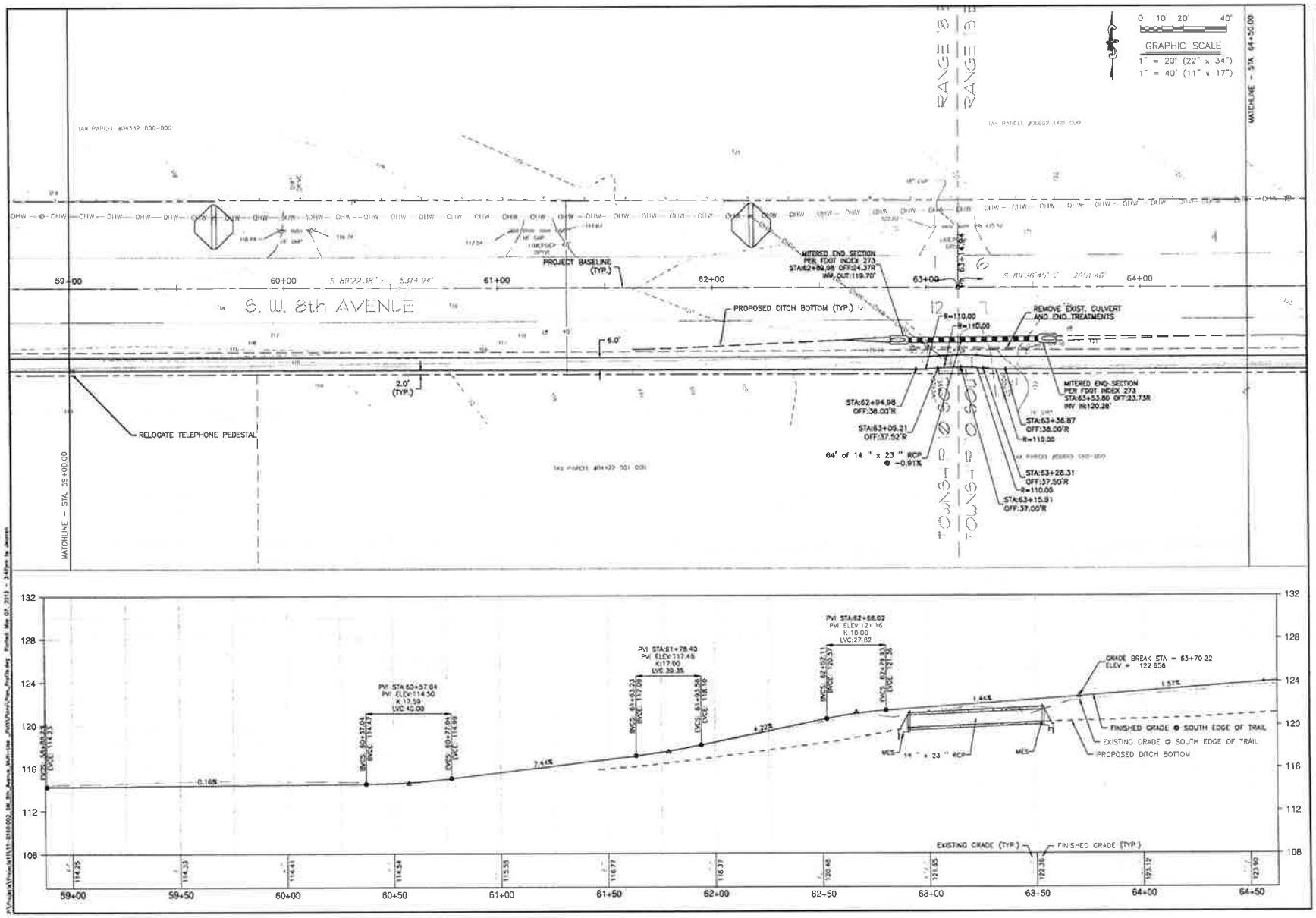
CHRISTOPHER S. TOMAC, P.E.
FLORIDA P.E. NO. 68628

PROJECT NO. 11-0160 002
SCALE: 1" = 20' (HORIZ) 1" = 4' (VERT) (CAL)
DATE: MAY 2013
DRAWING: 16



SITE ENGINEERING PLANS FOR

SW 8TH AVENUE
MULTI-USE PATH

ALACHUA COUNTY, FLORIDA

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PLAN & PROFILE
STA. 59+00 TO STA. 64+50

REVISIONS			
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DRAWN BY JPW
EXECUTED BY CDI
APPROVED BY CDI

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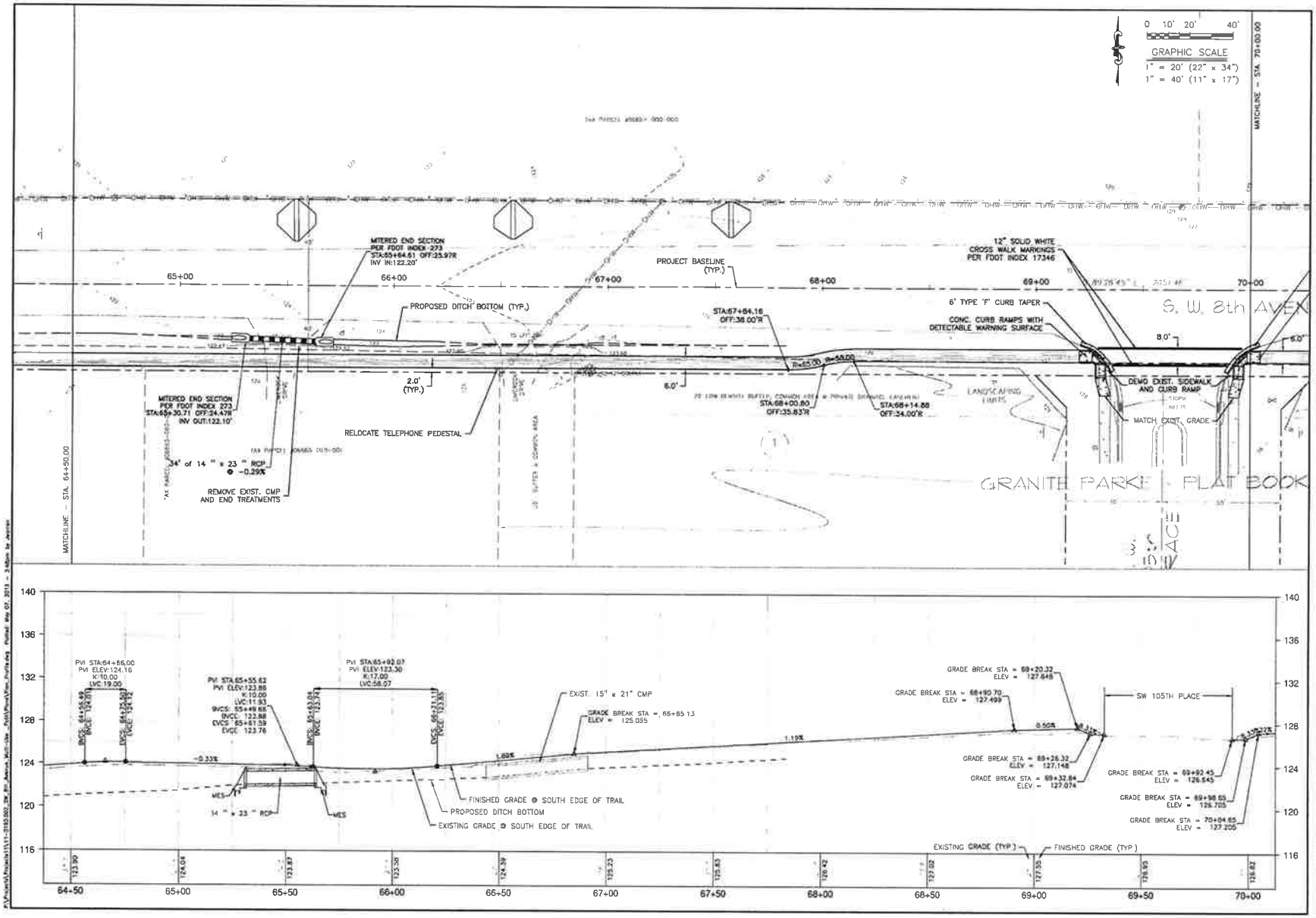
CHRISTOPHER S. TOMKO, P.E.
FLORIDA P.E. NO. 65508

PROJECT NO
11-0160.002

SCALE
1" = 20' (HORIZONTAL)
1" = 4' (VERTICAL)

DATE
MAY 2013

DRAWING
17



REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY	DRAWN BY	CHECKED BY	APPROVED BY
JPW	JPW	CDT	CDT

PLAN & PROFILE
STA. 64+50 TO STA. 70+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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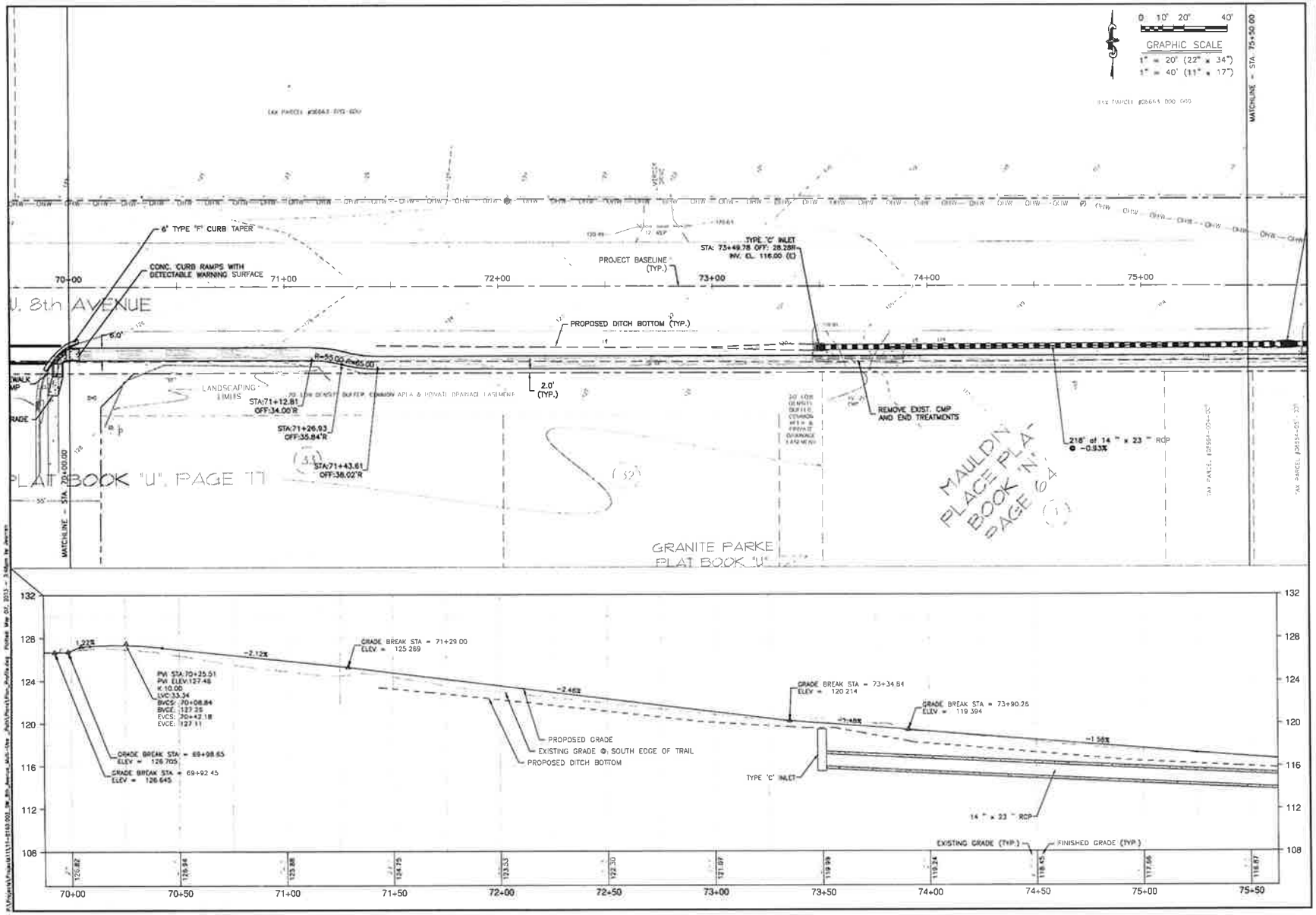
CHRISTOPHER D. TOWNE, P.E.
FLORIDA P.E. NO. 69829

PROJECT NO.: 11-0160.002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING: 18



REVISIONS			
NO	DATE	DESCRIPTION	BY

DESIGNED BY JPW
DRAWN BY JPW
CHECKED BY CDT
APPROVED BY CDT

PLAN & PROFILE
STA. 70+00 TO STA. 75+50

SITE ENGINEERING PLANS FOR:
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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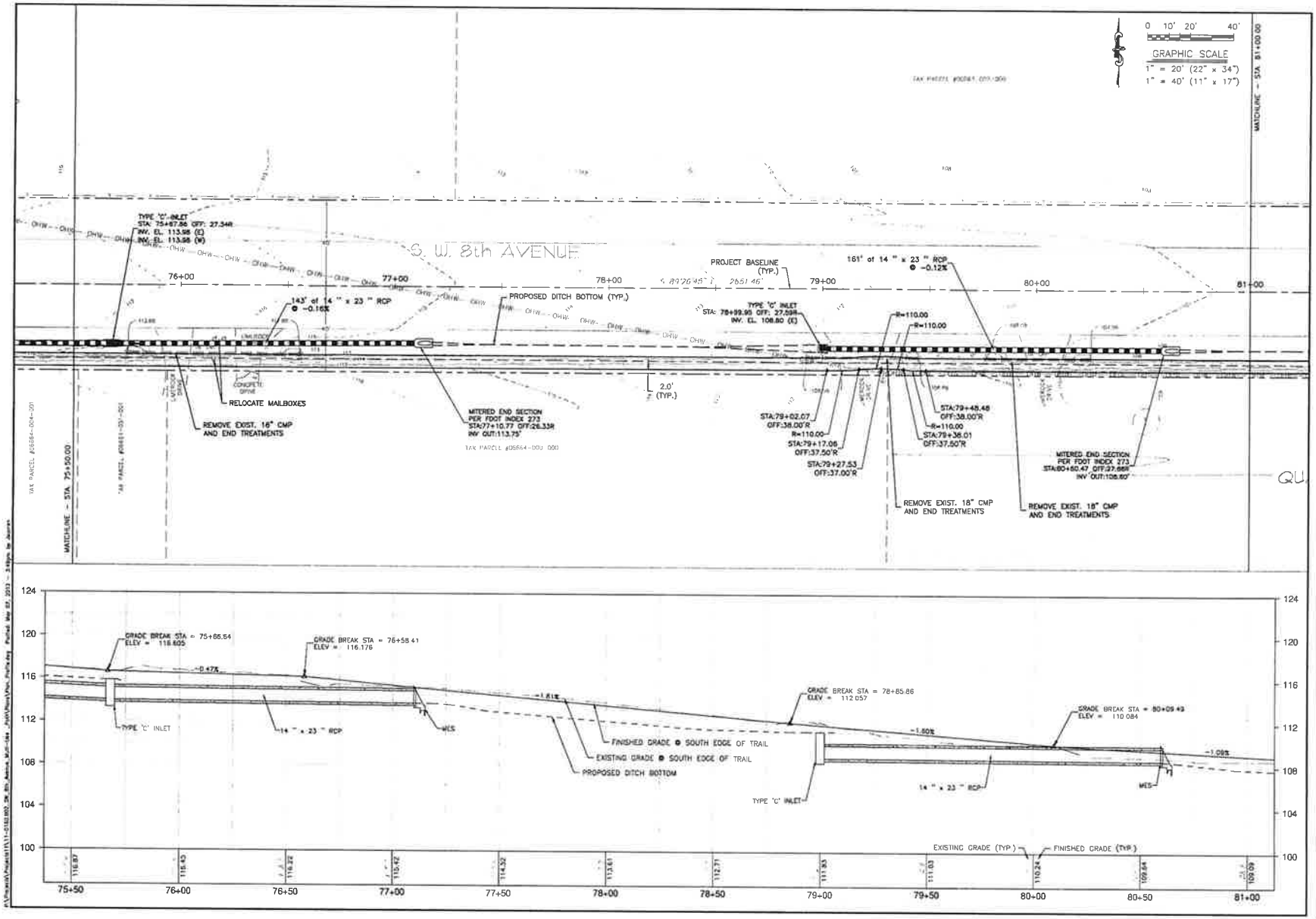
CHRISTOPHER D. TOWLE, P.E.
FLORIDA P.E. NO. 69919

PROJECT NO: 11-0160.002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING: 19



REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY JPW
DRAWN BY JPW
CHECKED BY CDT
APPROVED BY CDT

PLAN & PROFILE
STA. 75+50 TO STA. 81+00

SITE ENGINEERING PLANS FOR
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MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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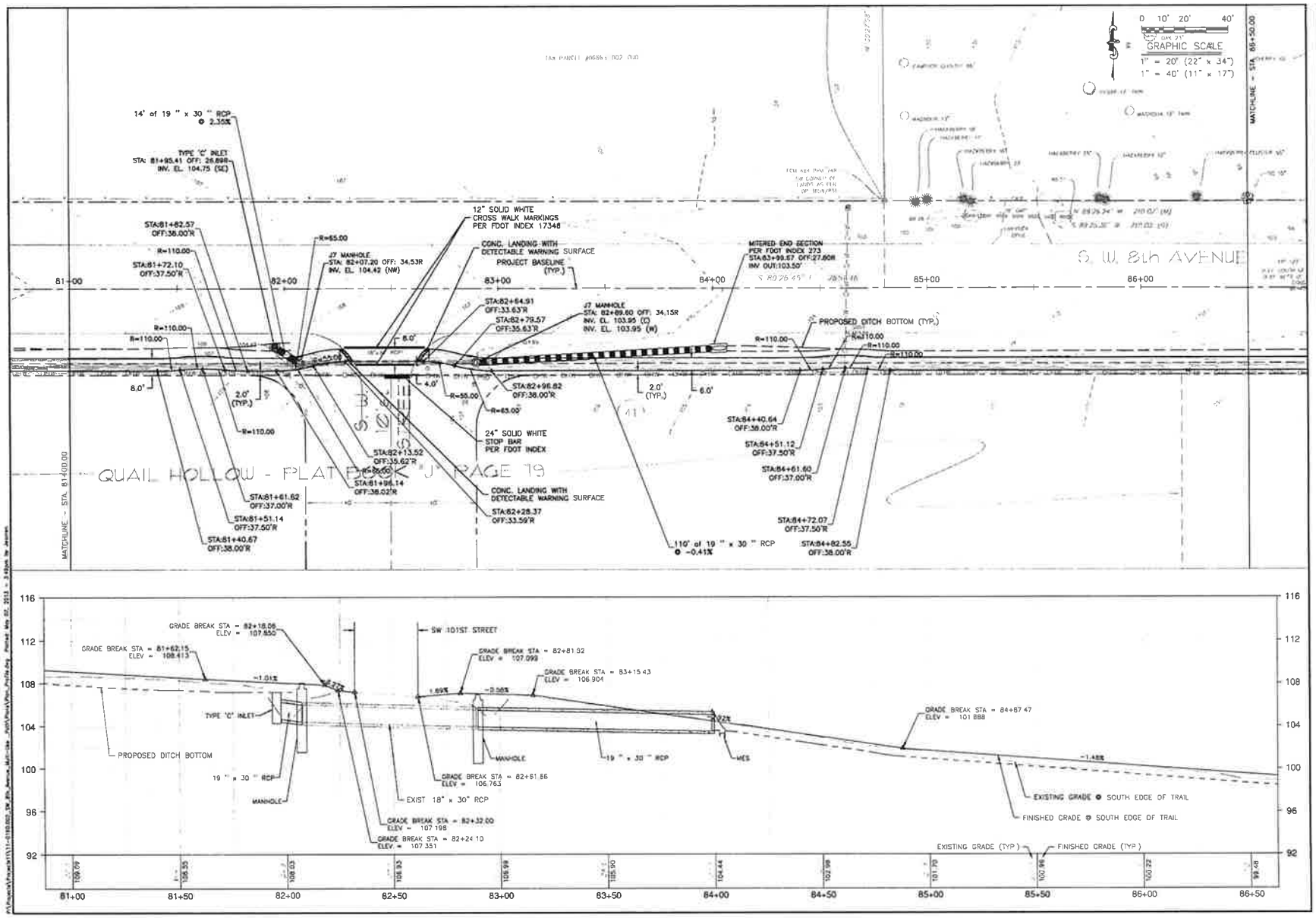
CHRISTOPHER B. TOMA, P.E.
FLORIDA P.E. NO. 60928

PROJECT NO 11-0160 002
SCALE 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE MAY 2013
DRAWING 20



REVISIONS			
NO.	DATE	DESCRIPTION	BY

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DRAWN BY	JTW	
CHECKED BY	CDT	
APPROVED BY	CDT	

PLAN & PROFILE
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SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
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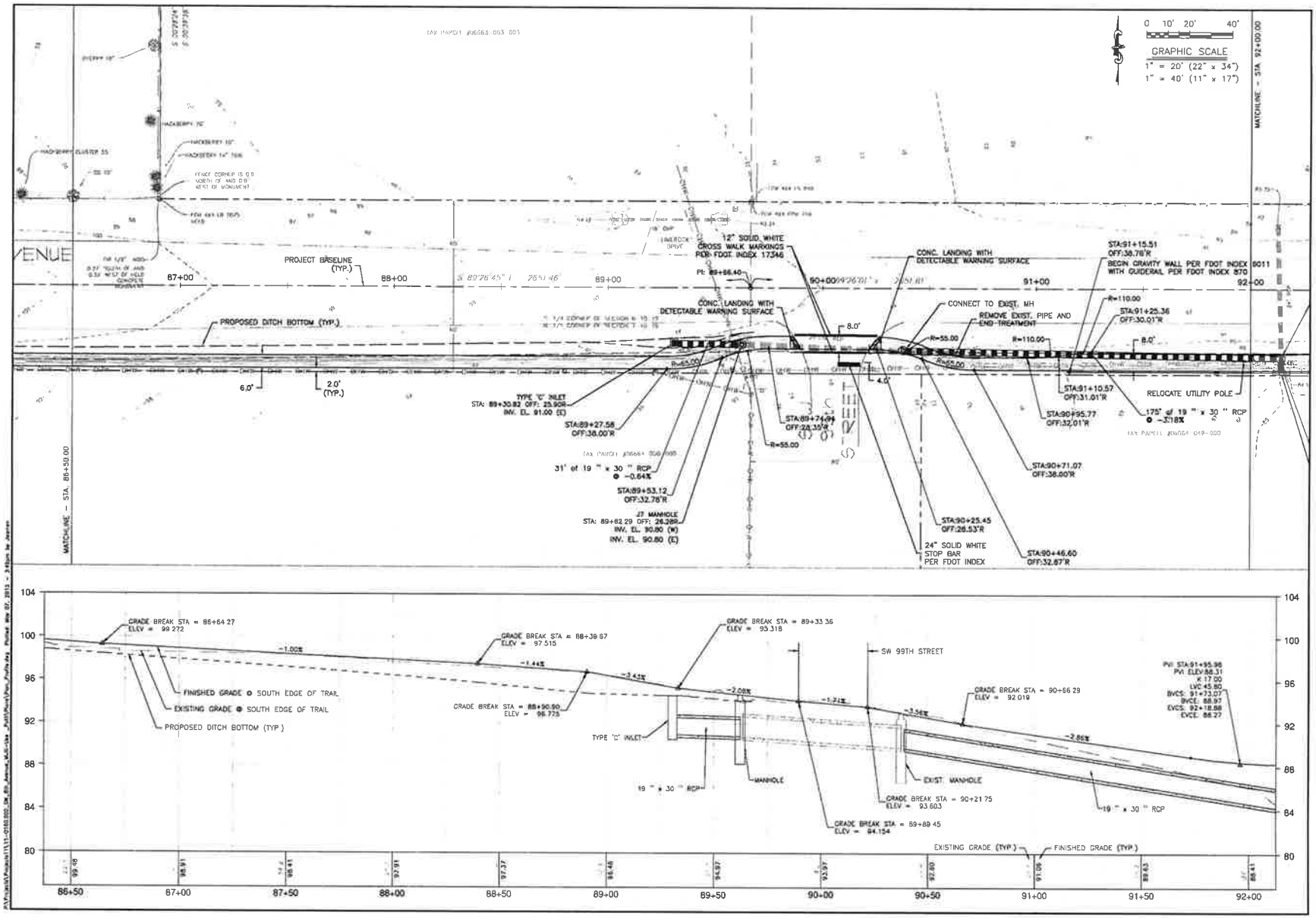
CHRISTOPHER S. TOWNE, P.E.
FLORIDA P.E. NO. 68828

PROJECT NO: 11-0160.002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING: 21



REVISIONS			
NO	DATE	DESCRIPTION	BY

DESIGNED BY JPW
DRAWN BY JPW
CHECKED BY CDT
APPROVED BY CDT

PLAN & PROFILE
STA. 86+50 TO STA. 92+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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64 SHEETS
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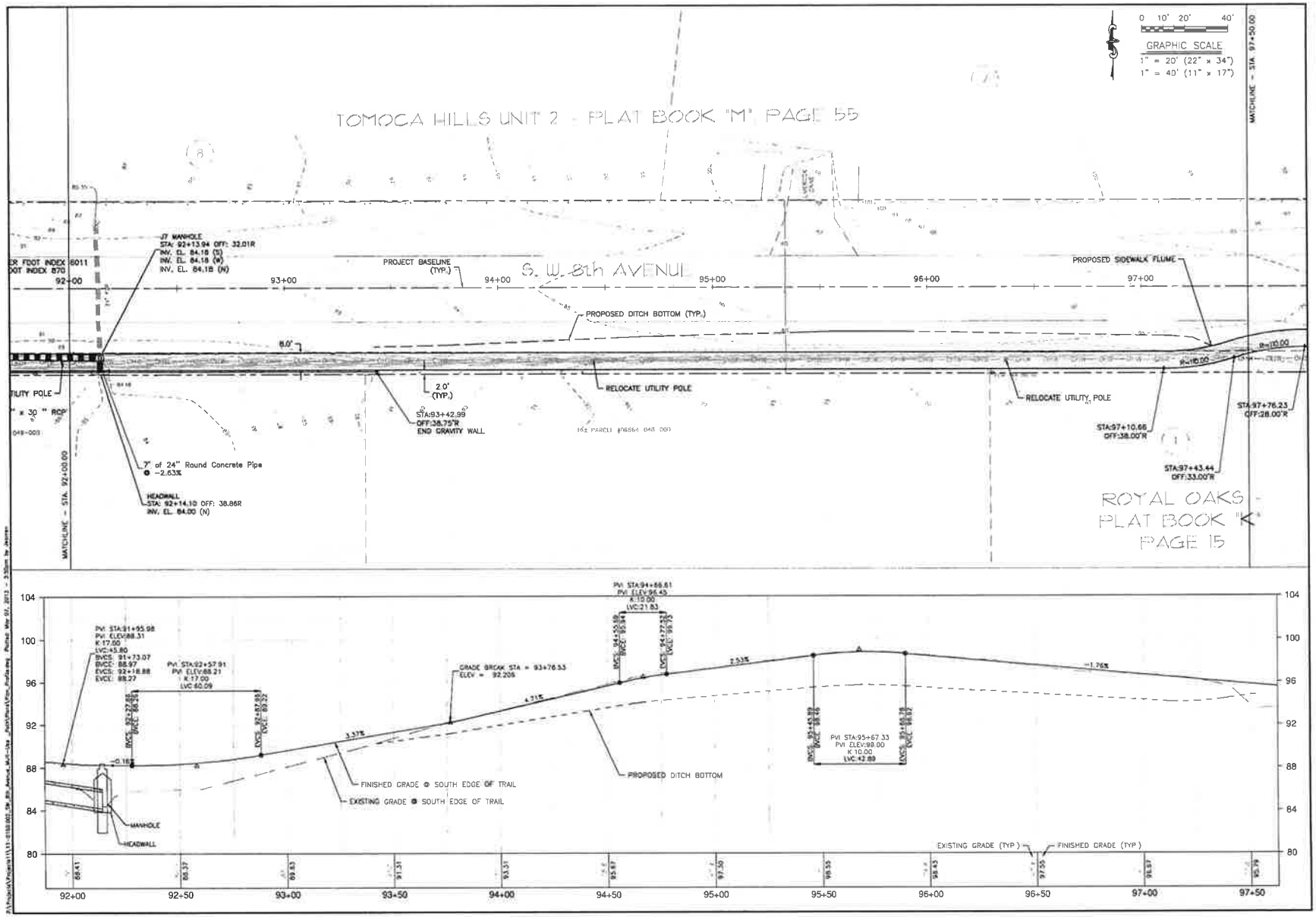
CHRISTOPHER D. TOMKO, P.E.
FLORIDA P.E. NO: 68828

PROJECT NO: 11-0160.002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING: 22



REVISIONS			
NO	DATE	DESCRIPTION	BY

DESIGNED BY: JTR
DRAWN BY: JTR
CHECKED BY: CDI
APPROVED BY: CDI

SITE ENGINEERING PLANS FOR:
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

PLAN & PROFILE
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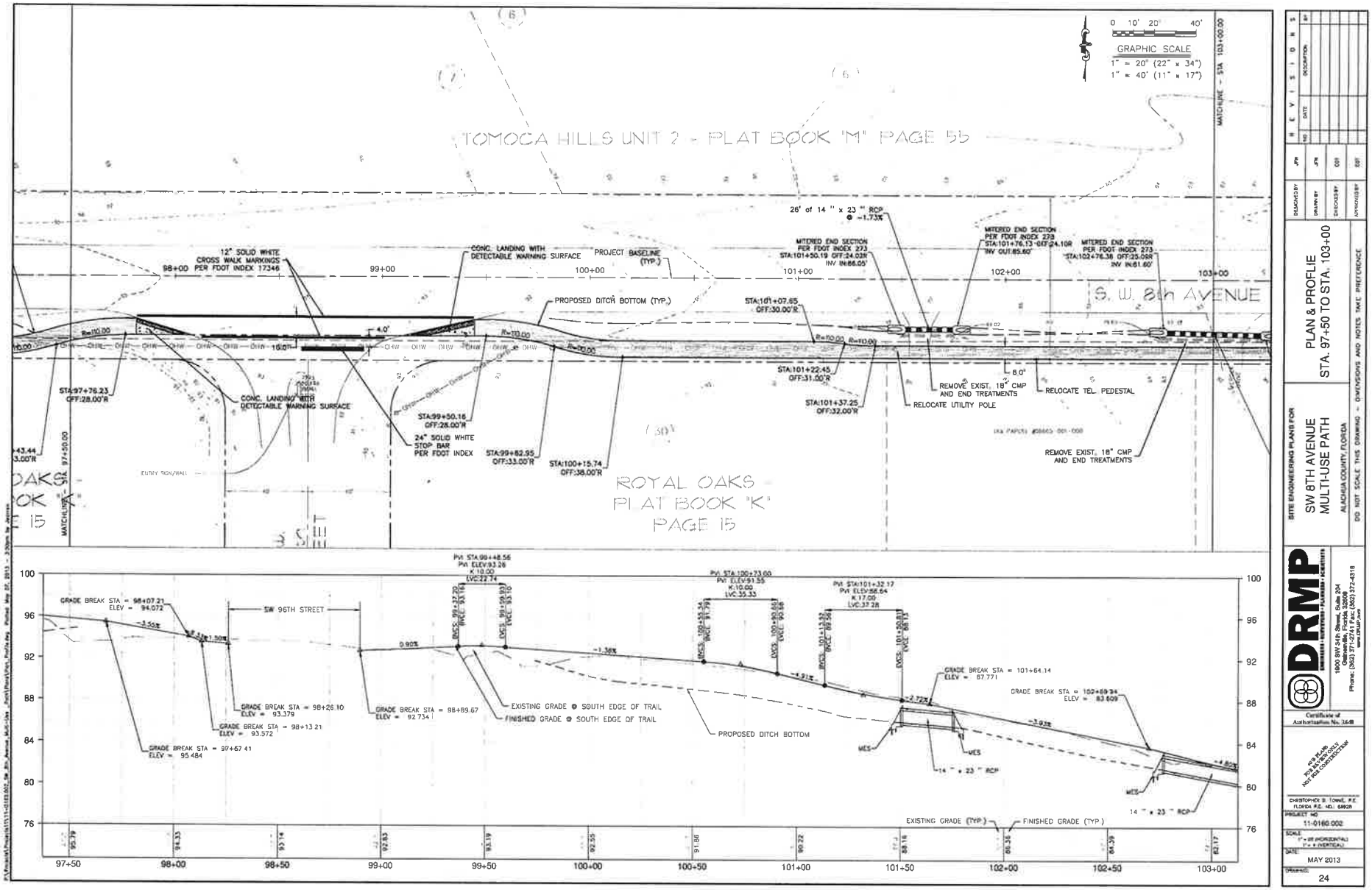
NOT A TRACING
NOT A SUBDIVISION

CHRISTOPHER S. TONGE, P.E.
FLORIDA P.E. NO. 69824

PROJECT NO: 11-0160 002
SCALE: 1" = 20' HORIZONTAL, 1" = 4' VERTICAL
DATE: MAY 2013
DRAWING NO: 23



REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY JPH
DRAWN BY JPH
CHECKED BY CGI
APPROVED BY CGI

PLAN & PROFILE
STA. 97+50 TO STA. 103+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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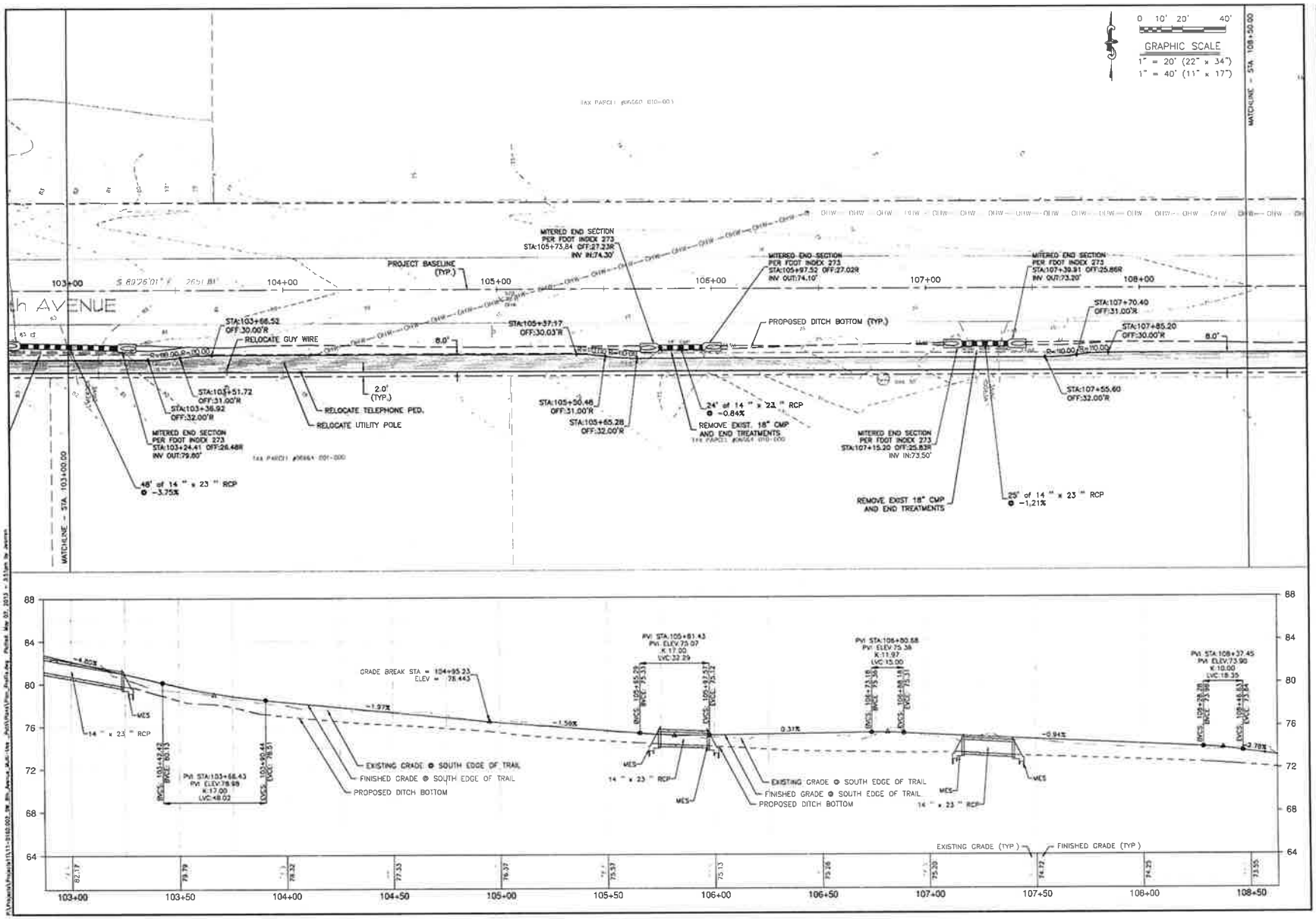
CHRISTOPHER S. TOWNE, P.E.
FLORIDA P.E. NO. 68928

PROJECT NO. 11-0160.002
SCALE 1" = 20' (HORIZONTAL) 1" = 4 (VERTICAL)
DATE MAY 2013
DRAWING 24



REVISIONS			
NO.	DATE	DESCRIPTION	BY

DESIGNED BY	JPW
DRAWN BY	JPW
CHECKED BY	CDI
APPROVED BY	CDI

PLAN & PROFILE
STA. 103+00 TO STA. 108+50

SITE ENGINEERING PLANS FOR:
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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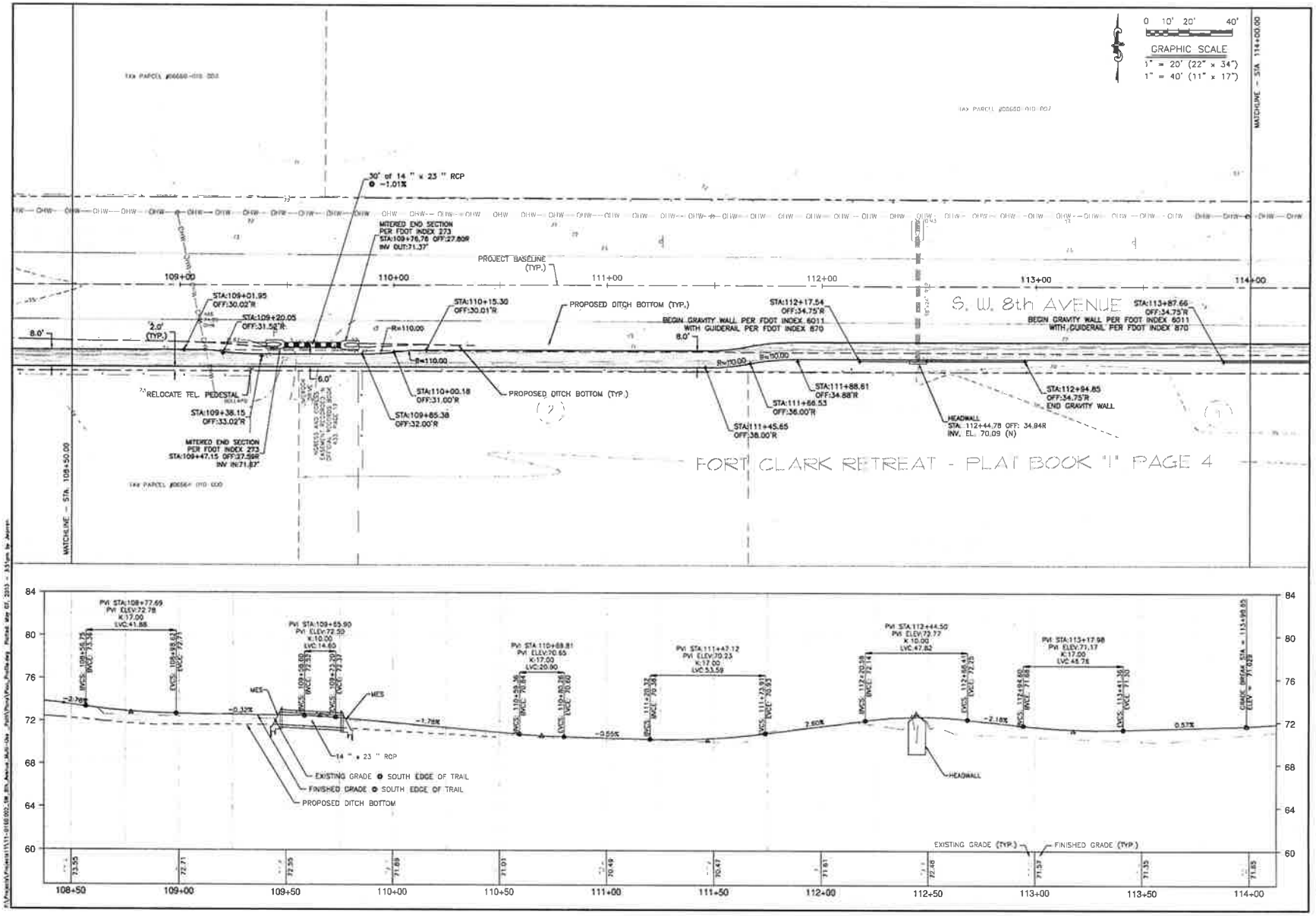
CHRISTOPHER D. TOMKE, P.E.
FLORIDA P.E. NO. 69928

PROJECT NO.
11-0160.002

SCALE:
1" = 20' (HORIZONTAL)
1" = 4' (VERTICAL)

DATE
MAY 2013

DRAWING:
25



REVISIONS			
NO	DATE	DESCRIPTION	BY

DESIGNED BY JFR
DRAWN BY JFR
CHECKED BY CDF
APPROVED BY CDT

PLAN & PROFILE
STA. 108+50 TO STA. 114+00

SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

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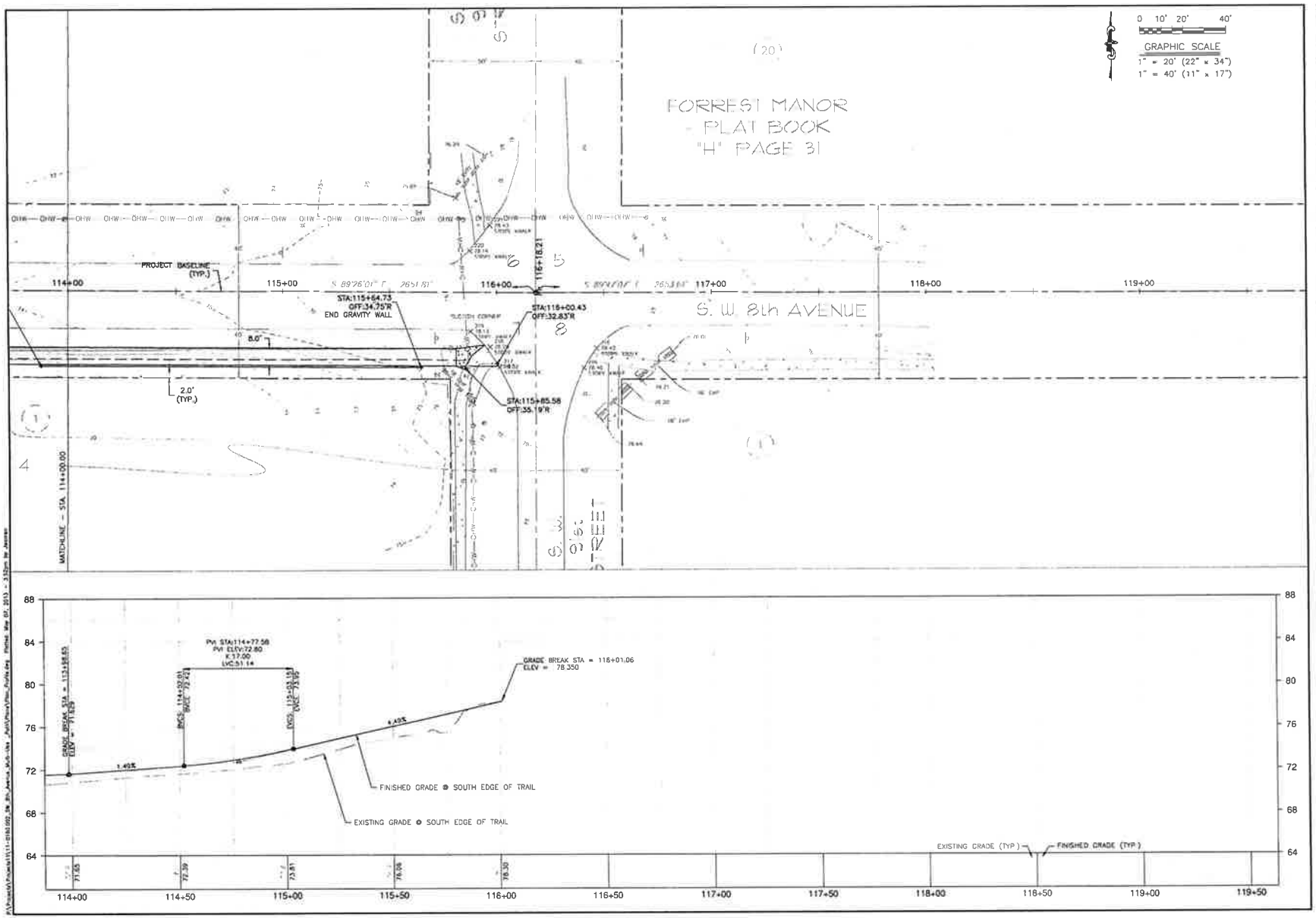
CHRISTOPHER D. TOWNE, P.E.
FLORIDA P.E. NO. 64634

PROJECT NO. 11-0160.002
SCALE: 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE: MAY 2013
DRAWING 26



SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
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ALACHUA COUNTY, FLORIDA

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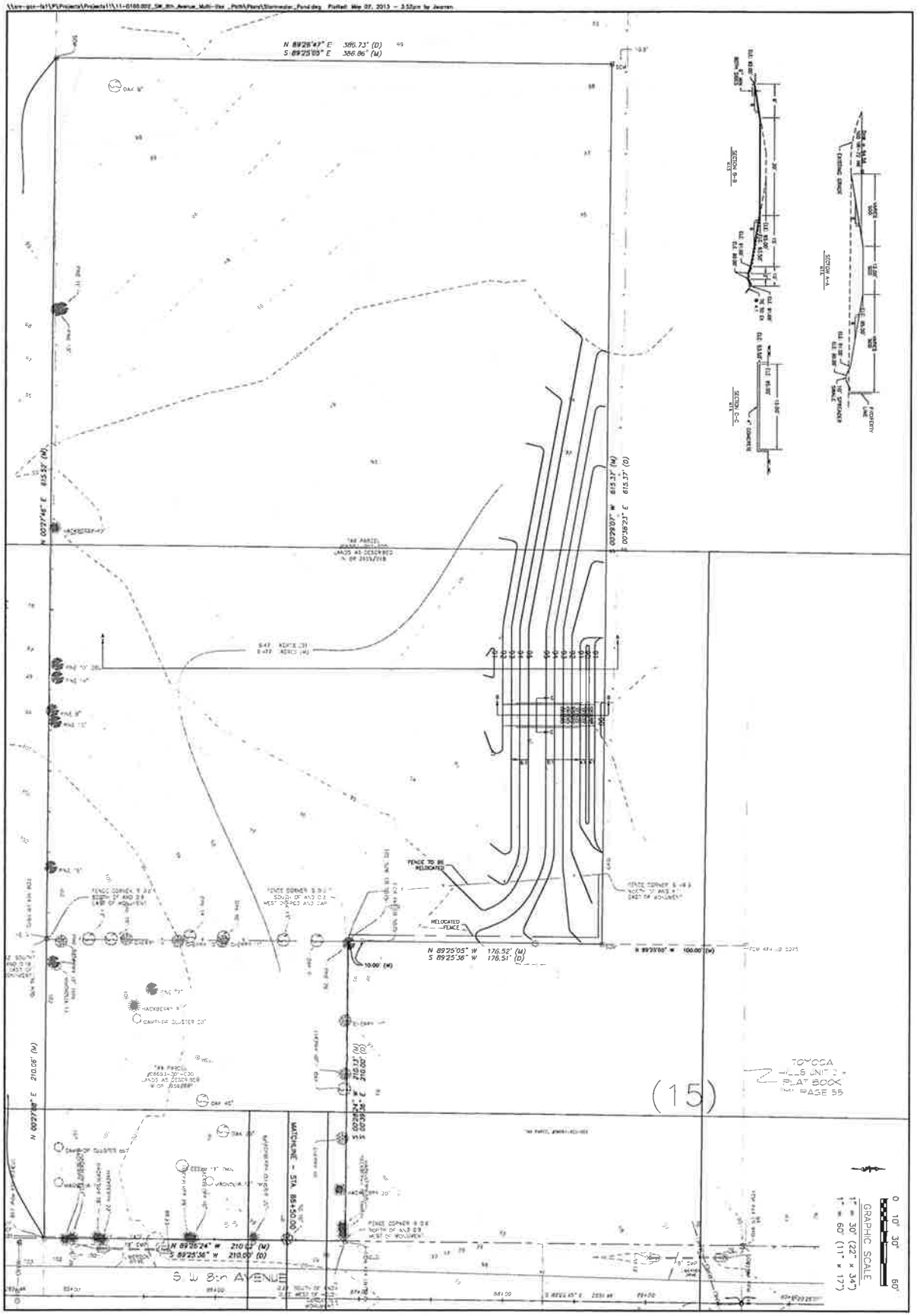
STEPHEN S. TOMIC, P.E.
FLORIDA P.E. NO. 68828

PROJECT NO 11-0160.002
SCALE 1" = 20' (HORIZONTAL) 1" = 4' (VERTICAL)
DATE MAY 2013
DRAWING 27



SHEET NO. 28	DATE MAY 2013	SCALE 1" = 30' (22" x 34") 1" = 60' (11" x 17")	PROJECT NO. 11-0160.002	CHRISTOPHER D. TONIE, P.E. FLORIDA REG. NO. 66630	Certificate of Authorization No. 3898

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SITE ENGINEERING PLANS FOR
SW 8TH AVENUE
MULTI-USE PATH
ALACHUA COUNTY, FLORIDA

PROPOSED
STORMWATER POND

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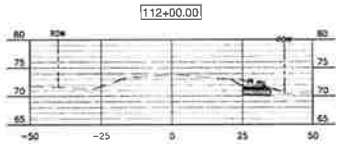
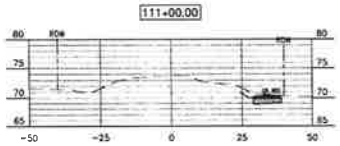
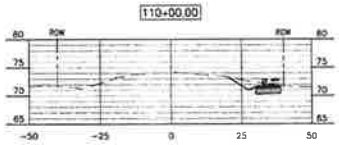
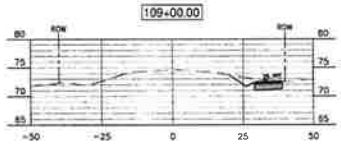
DRAWN BY	JFW
CHECKED BY	CDT
APPROVED BY	CDT

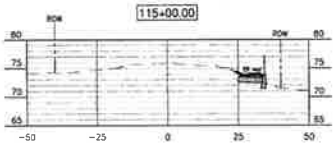
REVISIONS			
NO.	DATE	DESCRIPTION	BY

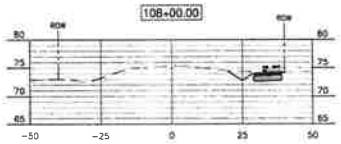


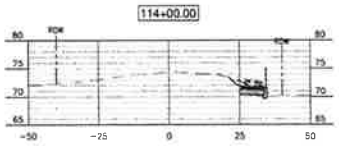


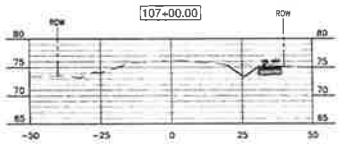


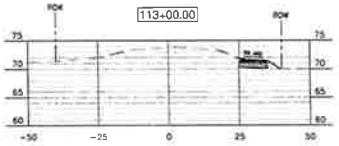












REVISIONS		
NO.	DATE	DESCRIPTION

DESIGNED BY	JTP
DRAWN BY	JPB
CHECKED BY	CDT
APPROVED BY	CDT

SITE ENGINEERING PLANS FOR:

SW 8TH AVENUE
MULTI-USE PATH

ALACHUA COUNTY, FLORIDA

CROSS SECTIONS
STA. 107+00 TO STA. 115+00

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PRELIMINARY NOT FOR CONSTRUCTION

CHRISTOPHER D. TOWLE, P.E.
FLORIDA P.E. NO. 69608

PROJECT NO.: 11-0160.002

SCALE: 1" = 50' (HORIZONTAL) 1" = 10' (VERTICAL)

DATE: MAY 2013

DRAWING 33

North Central Florida Regional Planning Council

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Transportation Disadvantaged Program - Coordinating Board Appointment

STAFF RECOMMENDATION

Appoint Dr. Maurice Levy as the voting Elderly Representative on the Alachua County Transportation Disadvantaged Coordinating Board.

BACKGROUND

This is regarding Florida's Transportation Disadvantaged Program established by Chapter 427, Florida Statutes. According to Rule 41-2.012 of the Florida Administrative Code, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area shall appoint members to the Alachua County Transportation Disadvantaged Coordinating Board.

The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area placed an advertisement to fill vacant positions on the Transportation Disadvantaged Coordinating Board in December 2012. In response to this advertisement, Dr. Levy's application was the only one received for the voting Elderly Representative position. Attached is Dr. Levy's application.

Attachment

t:\lynn\appt2013\alachua\apptml.docx

TRANSPORTATION DISADVANTAGED COORDINATING BOARD
MEMBERSHIP APPLICATION

Name: MAURICE LEVY

Address: 2281 NW 24th Avenue Gainesville FL 32605

Phone: 352 338 3591

E-Mail: MAURICE D LEVY

Date: 5/10/2013

County: ALACHUA

Representing (check position(s) you are applying for):

CITIZEN ADVOCATE ✓ Voting ✓ Alternate
Qualifications: A resident who is concerned about the needs of disadvantaged individuals.

CITIZEN ADVOCATE -USER ____ Voting ____ Alternate
Qualifications: A resident who uses the transportation disadvantaged system.

PERSONS WITH DISABILITIES REPRESENTATIVE ____ Voting ____ Alternate
Qualifications: A person who has a disability (examples - hearing impaired, visually impaired, mobility impaired) and/or represents persons with disabilities.

ELDERLY REPRESENTATIVE ✓ Voting ✓ Alternate
Qualifications: A person over sixty years of age representing the elderly in the county.

CHILDREN AT RISK ____ Voting ____ Alternate
Qualifications: A local representative for children at risk

MEDICAL COMMUNITY: ✓ Voting ✓ Alternate
Qualifications: A local representative of the medical community

PRIVATE TRANSIT INDUSTRY: ____ Voting ____ Alternate
Qualifications: An experienced representative of the local private for profit transportation industry.

Please complete and return to:

North Central Florida Regional Planning Council
2009 N.W. 67 Place
Gainesville, FL 32653-1603

Maurice D Levy

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

**SUBJECT: Transportation Disadvantaged Program-
Planning Grant Resolution, Fiscal Year 2013-14**

STAFF RECOMMENDATION

Approve the attached resolution authorizing the execution of the Fiscal Year 2013-14 Planning Grant Agreement for Alachua County.

BACKGROUND

This is regarding the Transportation Disadvantaged Program established by Chapter 427, Florida Statutes. The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area is the designated official planning agency for this program for Alachua County.

As the designated official planning agency, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area is responsible for Transportation Disadvantaged Program planning and providing the Transportation Disadvantaged Coordinating Board with sufficient staff support and resources to enable the Board to fulfill its responsibilities. The attached draft Resolution No. 2013-01 and Florida Commission for the Transportation Disadvantaged Planning Grant Agreement will provide the funding needed to provide staff services to the Coordinating Board.

Attachment

t:\lynn\pga\2014\pgaresmtpomemo.docx

RESOLUTION NO. 2013-01

A RESOLUTION OF THE METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA AUTHORIZING THE EXECUTION OF THE FISCAL YEAR 2013-14 TRANSPORTATION DISADVANTAGED TRUST FUND AGREEMENT WITH THE FLORIDA COMMISSION FOR THE TRANSPORTATION DISADVANTAGED; PROVIDING AN EFFECTIVE DATE

WHEREAS, the Federal Government, under the authority of 23 United States Code 134 and 49 United States Code 5303, requires each metropolitan area, as a condition to the receipt of federal capital or operating assistance, to have a continuing, cooperative and comprehensive transportation planning process that results in plans and programs consistent with the comprehensively planned development of the metropolitan area, and further requires the State Transportation Agency and the metropolitan area to enter into an Agreement clearly identifying the responsibilities of each party for cooperatively carrying out such transportation planning; and

WHEREAS, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has the authority to enter into the Fiscal Year 2013-14 Transportation Disadvantaged Trust Fund agreement and to undertake a transportation disadvantaged service project, as authorized by Section 427.0159, Florida Statutes and Rule 41-2, Florida Administrative Code.

NOW THEREFORE, BE IT RESOLVED BY THE METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION FOR THE GAINESVILLE URBANIZED AREA:

1. That the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has the authority to enter into the above referenced grant agreement and attached as Exhibit 1 and made part of this resolution by reference.

2. That the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area authorizes the Chairperson to execute the above referenced grant agreement on behalf of the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area with the Florida Commission for the Transportation Disadvantaged.

3. That the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area authorizes the Chairperson to sign any and all assurances, agreements or contracts that are required in connection with the agreement.

4. That the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area authorizes its Executive Director to act in connection with the agreement and to provide such additional information as may be required by the Florida Commission for the Transportation Disadvantaged.

5. That the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area authorizes its Executive Director to sign any and all reimbursement invoices, warranties, certification and any other documents that may be required in connection with the agreement.

6. That this resolution shall take effect upon its adoption.

DULY ADOPTED in regular session, this _____day of _____ A.D., 2013.

METROPOLITAN TRANSPORTATION
PLANNING ORGANIZATION FOR THE
GAINESVILLE URBANIZED AREA

_____
Randy Wells, Chair

ATTEST:

_____
Lauren Poe, Secretary

APPROVED AS TO FORM

_____
David C. Schwartz, Attorney
Metropolitan Transportation Planning Organization
for the Gainesville Urbanized Area

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Florida Commission for the

Transportation Disadvantaged

PROGRAM MANUAL

FOR THE

TRANSPORTATION DISADVANTAGED

PLANNING GRANT

Issued By:

FLORIDA COMMISSION FOR THE TRANSPORTATION DISADVANTAGED

605 Suwannee Street, Mail Station 49

Tallahassee, Florida 32399-0450

850-410-5700

Fax 850-410-5752

Internet http://www.dot.state.fl.us/ctd

INTRODUCTION

This manual contains information regarding the Transportation Disadvantaged Planning Grant Program administered by the Florida Commission for the Transportation Disadvantaged (Commission). It provides guidance to designated official planning agencies when implementing local transportation disadvantaged planning services under the State's Coordinated Transportation Disadvantaged Program.

The Transportation Disadvantaged Trust Fund is administered by the Commission, pursuant to Section 427.0159, Florida Statutes. The purpose of the Transportation Disadvantaged Trust Fund is to provide a dedicated funding source for the operational and planning expenses of the Commission in carrying out its legislative responsibilities, and to provide transportation opportunities for non-sponsored transportation disadvantaged citizens.

There are three parts to this manual: Program Requirements, Recipient Instructions, and Planning Grant Information Form. For the purpose of clarifying terms, the contract that will be returned for execution by eligible recipients is known as the "grant agreement."

PART I: PLANNING GRANT PROGRAM REQUIREMENTS

This part of the manual contains requirements that accompany the planning grant program and the tasks that are required to be accomplished.

1. PROJECT ELIGIBILITY

A. Eligible Activities

These grant funds allocated from the Transportation Disadvantaged Trust Fund are for the specific purpose of accomplishing the duties and responsibilities of the Designated Official Planning Agency as identified in Chapter 427, Florida Statute, Rule 41-2, Florida Administrative Code, Commission policies, and tasks as further described in this manual. Eligible expenditures are any costs incurred in carrying out the duties and responsibilities of the Designated Official Planning Agency or Metropolitan Planning Organization.

This is a lump sum – percent complete grant to accomplish the tasks identified in Chapter 427 FS; Rule 41-2 FAC; this Program Manual; and the grant agreement. It is not subject to adjustment due to the actual cost experience of the recipient in the performance of the contract. You will be paid based on the weighted value of the tasks and deliverables that have been accomplished for the invoiced period. Prior to

payment, the tasks performed and deliverables are subject to review and acceptance by the Commission. The criteria for acceptance of completed tasks and deliverables are based on the most recent regulations, guidelines or directives related to the particular task and deliverable. Recipients will be paid for the satisfactory performance of services detailed below. Recipients may not use these grant funds to supplant or replace planning funds for transportation disadvantaged services which are currently provided to a recipient by any federal, state, or local governmental agency. Specific required tasks are as follows:

TASK 1: **Weighted value= 17%**

Jointly develop and annually update the Transportation Disadvantaged Service Plan with the community transportation coordinator and the local coordinating board.

Deliverable: Complete Transportation Disadvantaged Service Plan or annual updates. Due prior to the end of grant agreement period (June 30) and pursuant to the Commission's latest instructions for the Memorandum of Agreement and the Transportation Disadvantaged Service Plan.

TASK 2: **Weighted value= 15%**

A. When necessary and in cooperation with the local coordinating board, solicit and recommend a community transportation coordinator, in conformity with Chapters 287 and 427, Florida Statutes. Such recommendation shall be presented to the Commission by Planning Agency staff or their designee as needed

OR

B. Provide staff support to the local coordinating board in conducting an annual evaluation of the community transportation coordinator, including local developed standards as delineated in the adopted Transportation Disadvantaged Service Plan. Assist the Commission for the Transportation Disadvantaged in joint reviews of the community transportation coordinator.

Deliverable:

A. Planning Agency's CTC recommendation letter and signed resolution from the Planning Agency.

B. LCB and Planning Agency selected CTC evaluation worksheets pursuant to the most recent version of the Commission's CTC Evaluation Workbook. (at a minimum, addressing Competition, Cost-Effectiveness and Efficiency, and Level of Coordination).

TASK 3: **Weighted value= 40%**

Organize and provide staff support and related resources for at least four (4) local coordinating board meetings per year, holding one meeting during each quarter.

LCB meetings will be held in accordance with the Commission's most recent Local Coordinating Board and Planning Agency Operating Guidelines and will include at least the following:

1. Agendas for local coordinating board meetings. Where applicable, operator payments should be addressed as a standard agenda item for each meeting.

2. Official minutes of local coordinating board meetings and committee meetings (regardless of a quorum). A copy will be submitted along with the quarterly report to the Commission. Minutes will at least be in the form of a brief summary of basic points, discussions, decisions, and recommendations. Records of all meetings shall be kept for at least five years.

3. A current full and active membership of voting and non-voting members to the local coordinating board. Any time there is a change in the membership, provide the Commission with a current membership roster and mailing list of local coordinating board members.

4. A report of the LCB membership's attendance at the last 4 consecutive LCB meetings (not committee's).

Provide staff support for committees of the local coordinating board.

Provide public notice of local coordinating board meetings in accordance with the most recent Local Coordinating Board and Planning Agency Operating Guidelines.

Provide program orientation and training for newly appointed local coordinating board members.

Deliverable: LCB Meeting agendas; minutes; membership roster; attendance report; public notice of meetings; training announcement and agenda.

TASK 4: **Weighted value=4%**

Provide at least one public hearing annually by each local coordinating board, and assist the Commission, as requested, in co-sponsoring public hearings. This public hearing must be held separately from the local coordinating board meeting. It may, however, be held on the same day as the scheduled local coordinating board meeting (immediately following or prior to the local coordinating board meeting).

Deliverable: Public Hearing agenda and minutes of related hearing only. The agenda and minutes should be separate documents and should not be included in the local coordinating board meeting agenda and minutes, if held on the same day. Minutes may reflect "no comments received" if none were made.

TASK 5: **Weighted value=4%**
Develop and annually update by-laws for local coordinating board approval.

Deliverable: Copy of LCB approved By-Laws with date of update noted on cover page.

TASK 6: **Weighted value=4%**
Develop, annually update, and implement local coordinating board grievance procedures in accordance with the Commission's most recent Local Coordinating Board and Planning Agency Operating Guidelines. Procedures shall include a step within the local complaint and/or grievance procedure that advises a dissatisfied person about the Commission's Ombudsman Program.

Deliverable: Copy of LCB approved Grievance Procedures with date of update noted on cover page.

TASK 7: **Weighted value=4%**
Review and comment on the Annual Operating Report for submittal to the local coordinating board, and forward comments/concerns to the Commission for the Transportation Disadvantaged.

Deliverable: Cover Page of Annual Operating Report, signed by LCB Chair.

TASK 8: **Weighted value=4%**
Research and complete the Actual Expenditures Report for direct federal and local government transportation funds to the Commission for the Transportation Disadvantaged no later than September 15th. Complete the Actual Expenditure Report, using the Commission approved forms.

Deliverable: Complete Actual Expenditure Report in accordance with the most recent Commission's instructions.

TASK 9: **Weighted value=4%**
Develop and provide the local coordinating board with quarterly progress reports of transportation disadvantaged planning accomplishments and planning contract deliverables as outlined in the planning grant agreement and any other activities related to the transportation disadvantaged program, including but not limited to, consultant contracts, special studies, and marketing efforts.

Deliverable: Complete Quarterly Progress Reports submitted with invoices.

TASK 10: **Weighted value=4%**

Attend at least one Commission sponsored training, including but not limited to, the Commission's regional meetings, the Commission's annual training workshop, or other sponsored training.

Deliverable: Documentation related to attendance at such event(s).

B. Eligible Service Areas

The eligible service area is that area(s) for which a Designated Official Planning Agency or Metropolitan Planning Organization has been specifically designated by the Commission for the Transportation Disadvantaged to serve. Planning grant agreements will only be approved for funding amounts which are within the allocation for the respective service area(s).

C. Eligible Recipients

An eligible recipient is any official body, agency or entity designated by the Commission for the Transportation Disadvantaged to fulfill the functions associated with staffing the Local Coordinating Board and other necessary local designated planning agency functions. The Metropolitan Planning Organization (MPO) shall serve as the Designated Official Planning Agency in areas covered by such organizations unless the CTD has designated a service area beyond the area for which an MPO has been created to serve. In designated service areas not covered by a Metropolitan Planning Organization, agencies eligible for selection as the designated planning agency include county or city governments, Regional Planning Councils, local planning organizations or other planning providers who are currently performing planning activities in designated service areas or capable of such.

To be eligible for any payment under in this grant agreement, there must first be a functioning Local Coordinating Board in the respective service area. In other words you cannot selectively perform some planning tasks without successfully supporting a functioning local coordinating board first. The determination of whether a Local Coordinating Board is functioning will be based on supportive documentation in the Commission files.

2. TRUST FUND ALLOCATION

Each year, on or around December 15th, the CTD will calculate each service area's allocation in accordance with 41-2 FAC. Each service area's anticipated eligible allocation is subject to change based on appropriations by the Legislature.

3. UNOBLIGATED ALLOCATIONS

Planning funds initially allocated to a county for which a grant agreement has not been executed by October 1 will no longer be available to that county.

4. TRANSFER OF PLANNING ALLOCATION

The portion of the Transportation Disadvantaged Trust Fund allocated for planning grants that is not needed by eligible Designated Official Planning Agencies or Metropolitan Planning Organizations and is unobligated in a grant agreement, may be transferred to each respective county's non-sponsored trip/equipment related allocation. A 10 percent local match will be required for all transferred planning dollars. Please notify the Commission by June 1, of any intent to do so.

Any Designated Official Planning Agency or Metropolitan Planning Organization which does not intend to use all of its allocated planning funds should notify the respective Community Transportation Coordinator so that the Coordinator can incorporate the additional allocation in their initial Trip and Equipment grant processing and make provisions to acquire the necessary local match. This is intended to occur prior to the execution of either grant agreement within the same grant cycle.

5. GRANT AGREEMENT APPROVAL

All grants are subject to approval by the Commission or its designee. Once the grant has been approved, a grant agreement will be forwarded to the recipient for execution.

Costs incurred prior to grant agreement execution cannot be charged to the project, nor will the Commission give retroactive grant agreement execution. Costs incurred by a recipient to process this agreement are not eligible project costs for this project.

Upon collection of necessary information from the Commission project manager, a grant agreement will be emailed to the recipient for execution and return. The grant agreement must be executed promptly in accordance with its accompanying instructions upon grant approval. The grantee should advise the Commission immediately if the grant agreement cannot be executed within 90 days after receipt of the agreement.

Invoices for Transportation Disadvantaged Trust Funds will not be honored until the grant agreement has been executed by both the Commission and the grantee, and is on file at the Commission office. Additionally, required invoice summary forms must be used when requesting reimbursement.

6. PROGRAM MANAGEMENT

General: Grantees must administer grants following the guidelines and procedures identified in the Federal Common Grant Rule except where specifically stated otherwise.

This document contains information concerning audits, allowable costs, accounting procedures, records retention, cost allocation plans, and other financial management requirements.

Other important topics including but not limited to: third party contracts; audits; monitoring; records retention; reporting; invoicing; reimbursement; vendors and subcontractors rights; are contained in the Grant Agreement.

PART II: GRANT AGREEMENT INSTRUCTIONS

GENERAL INSTRUCTIONS

Presented in this part are instructions in preparing for the execution of the grant agreement. To be considered you must contact your CTD Project Manager and provide or update certain information as addressed later in this part.

NOTE: Remember it is the Planning Agency's responsibility to contact their CTD Project Manager and provide related information necessary to draft the grant agreement, and return the grant agreement in a timely manner.

The Commission requires county-by-county accountability for all planning grants unless the Commission has designated a multi-county service area, and you have been designated as the planning agency for that area. Most deliverables from a designated multi-county service area are singular in nature as they treat the multi-county area as one area with a combined fixed price budget. If you are a designated planning agency for more than one service area, your grant agreement and all its deliverables will be required for each service area you have been designated to serve. However, your planning contract will identify separate budget pages and their respective task line items in the budget for each service area.

A. Although specific instructions on the preparation of the grant agreement are provided, additional assistance may be obtained by contacting the Commission for the Transportation Disadvantaged, (850) 410-5700.

B. If you are an existing designated official planning agency you can simply notify your area Project Manager whether any of the organizational information, address, or Federal ID number has changed from your last grant cycle. You can review the attached form titled Planning Grant Information Form to see what kind of information we are looking to update. If you are a first time grant recipient, you should complete the form and provide it to your Project Manager.

C. Upon receipt of the grant agreement, print out, execute and mail two (2) original signature copies of the grant agreement the grant agreement to the:

Commission for the Transportation Disadvantaged
605 Suwannee Street, Mail Station 49
Tallahassee, Florida 32399-0450

D. A resolution by your governing body addressing the authority to enter into the grant agreement is mandatory. Make sure it accompanies your executed grant agreement.

E. TIMETABLE

JULY 1	Earliest month that grant agreements can be effective for these grant funds. CTD's fiscal year begins on July 1. Contracts not executed prior to July 1 will begin on the date of execution.
JUNE 30	All Grant Agreements will terminate on June 30th the following year.

TRANSPORTATION DISADVANTAGED PLANNING GRANT INFORMATION FORM INSTRUCTIONS

Except for the following notes, the subject information form is essentially self-explanatory. If questions arise, please contact the Commission.

GRANT RECIPIENT LEGAL NAME: The full legal name of the grant recipient's organization, not an individual. Name must match Federal ID number and the information registered with MyFloridaMarketPlace.

FEDERAL IDENTIFICATION NUMBER: The number used by all employers within the United States to identify their payroll and federal income tax. Name must match Federal ID number and the information registered with MyFloridaMarketPlace.

REGISTERED ADDRESS: This should be the grant recipient's mailing address as registered in MyFloridaMarketPlace, and will be the address on the grant agreement. This address should also be consistent with the address associated with your Federal Employer Identification (FEI) Number. The grant recipient should notify the Commission and MyFloridaMarketPlace when an address change occurs.

PHONE NUMBERS AND E-MAIL ADDRESS: To facilitate faster communications, the grant recipient should also include telephone, fax numbers and any e-mail addresses used by the grant recipient. Inclusion of an e-mail address is **mandatory** for receipt of the grant agreement.

PROJECT START DATE: The start date shall be July 1st.

If you do not have an active local or multi-county Coordinating Board in place at this time, you are ineligible to receive these funds. An active local or multi-county Coordinating Board is determined by documented activities of the Local Coordinating Board and the planning agency that is on file with the Commission at the time of submission.

AUTHORIZING RESOLUTION INSTRUCTIONS

A resolution authorizing an individual and/or position to sign the grant agreement and subsequent agreements, invoices, assurances, etc. must be completed by the grant recipient's board of directors. A sample resolution is provided for your consideration. It is not required that you use the sample resolution, as long as similar basic information is provided. The resolution must include original signatures. Remember that the resolution can be good for an extended period or for multiple contracts if worded accordingly.

PART III

CONTRACTING FORMS

Transportation Disadvantaged Planning Grant Information Form

GRANT RECIPIENT LEGAL NAME:_____

FEDERAL IDENTIFICATION NUMBER:_____

REGISTERED ADDRESS:_____

CITY AND STATE:_____ ZIP CODE:_____

CONTACT PERSON FOR THIS GRANT:_____

PHONE NUMBER:_____ FAX NUMBER:_____

(**REQUIRED**) E-MAIL ADDRESS:_____

PROJECT LOCATION [County(ies)]:_____

PROPOSED PROJECT START DATE:_____ ENDING DATE:_____

PLANNING FUNDS TRANSFERRED TO TRIP & EQUIPMENT GRANT	GRANT AMOUNT REQUESTED FOR THIS CONTRACT PERIOD
$	$

I _____, as the authorized Grant Recipient Representative, hereby certify that the information contained in these forms is true and accurate and is submitted in accordance with the instructions.

_____ _____

Grant Recipient Representative (Signature) Date

FORM TO REQUEST TRANSFER OF PLANNING GRANT FUNDS TO CTC

<u>DO NOT COMPLETE</u> THIS FORM UNLESS YOU PLAN TO TRANSFER ALL OR A PORTION OF YOUR ALLOCATED PLANNING AGENCY MONEYS TO THE CTC DURING THIS GRANT CYCLE PLEASE COMPLETE THE FOLLOWING FORM AND SUBMIT TO THE COMMISSION.

Date: _____

To: Commission for the Transportation Disadvantaged
Attention: Project Manager

Fax #: 850-410-5751 or 850-410-5752

From: _____

Phone Number: _____

Planning Agency _____

County: _____
Only One County Per Form

PLANNING FUND ALLOCATION TRANSFERRED TO TRIP & EQUIPMENT GRANT

State $_____

SAMPLE
AUTHORIZING RESOLUTION

A RESOLUTION of the BOARD OF DIRECTORS of the _____ (Recipient), hereinafter BOARD, hereby authorizes the execution of a Transportation Disadvantaged Trust Fund Grant Agreement with the Florida Commission for the Transportation Disadvantaged.

WHEREAS, this BOARD is eligible to receive a Transportation Disadvantaged Trust Fund Grant and to undertake a transportation disadvantaged planning service project as authorized by Section 427.0159, Florida Statutes, and Rule 41-2, Florida Administrative Code.

NOW, THEREFORE, BE IT RESOLVED BY THE BOARD THAT:

1. The BOARD has the authority to file this grant agreement.

2. The BOARD authorizes _____ to execute the grant agreement on behalf of the _____ with the Florida Commission for the Transportation Disadvantaged.

3. The BOARD'S Registered Agent in Florida is_____ The Registered Agents address is: _____.

4. The BOARD authorizes _____ to sign any and all agreements or contracts which are required in connection with the application.

5. The BOARD authorizes _____ to sign any and all assurances, reimbursement invoices, warranties, certifications and any other documents which may be required in connection with the application or subsequent agreements.

DULY PASSED AND ADOPTED THIS _____ DAY OF_____
BOARD OF _____

_____
Typed name of Chairperson

_____
Signature of Chairperson

ATTEST: _____
Signature

North Central Florida Regional Planning Council

Serving
Alachua • Bradford
Columbia • Dixie • Gilchrist
Hamilton • Lafayette • Madison
Suwannee • Taylor • Union Counties

2009 NW 67th Place, Gainesville, FL 32653-1603 • 352.955.2200

May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Transportation Disadvantaged Program – Status Report

STAFF RECOMMENDATION

No action required. This agenda item is for information only.

BACKGROUND

Attached are the following reports:

1. Alachua County Transportation Disadvantaged Service Plan Standards Report shows that:

   - MV met the on-time performance standard;
   - MV met the complaint standard;
   - MV met the call hold time standard;
   - MV did not meet the accident standard in February 2013; and
   - MV met the roadcall standard.

2. MV Transportation Operations Report June 2013 - March 2013.

Attachments

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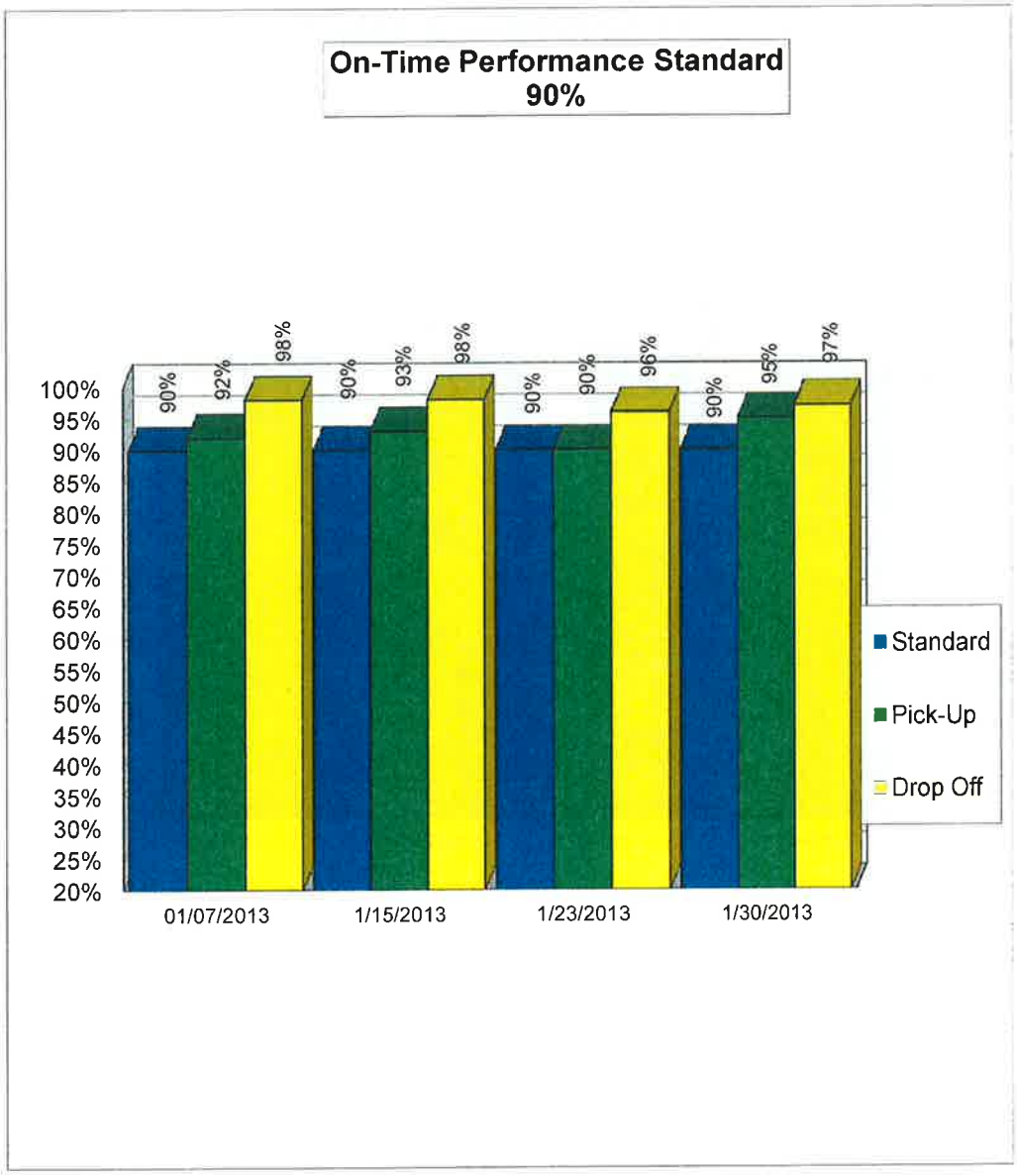
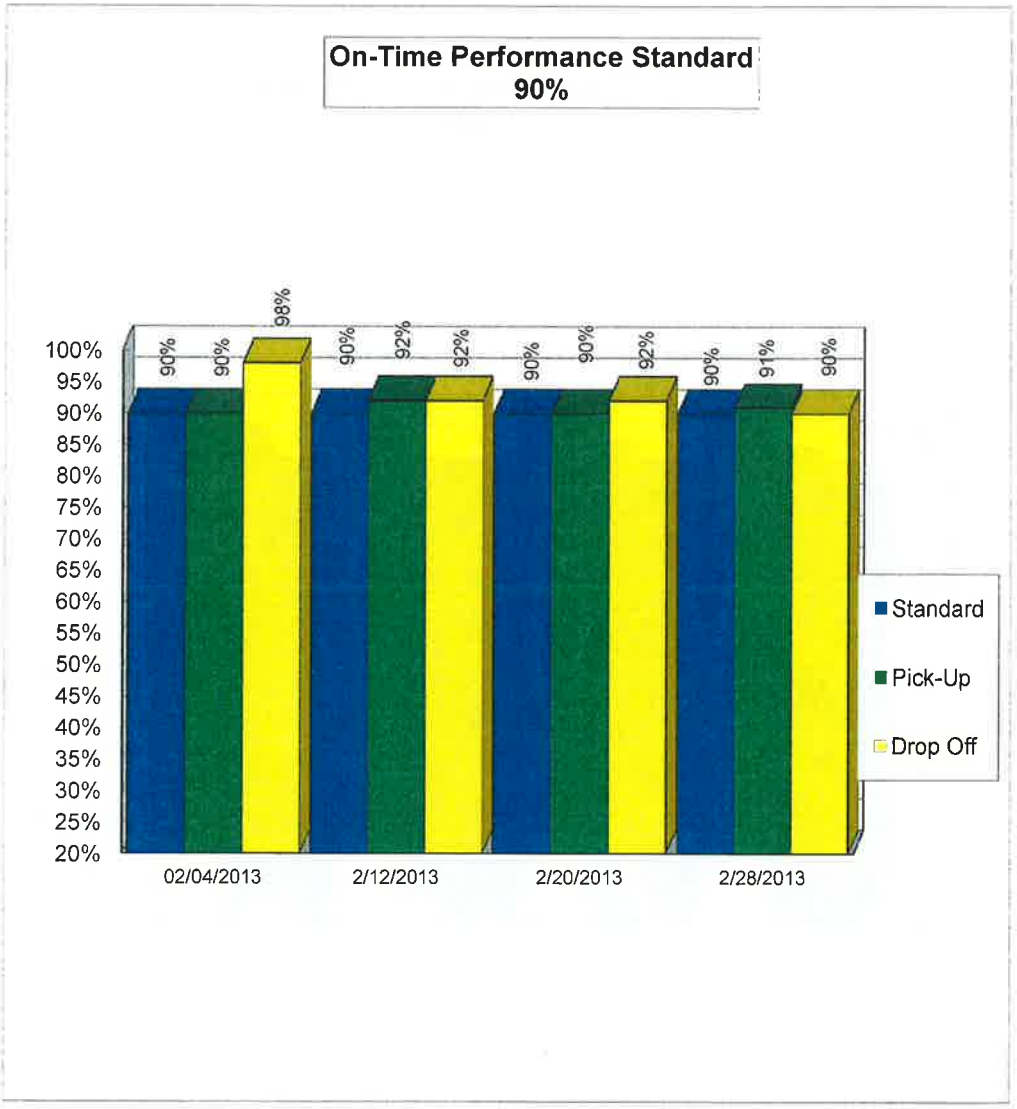
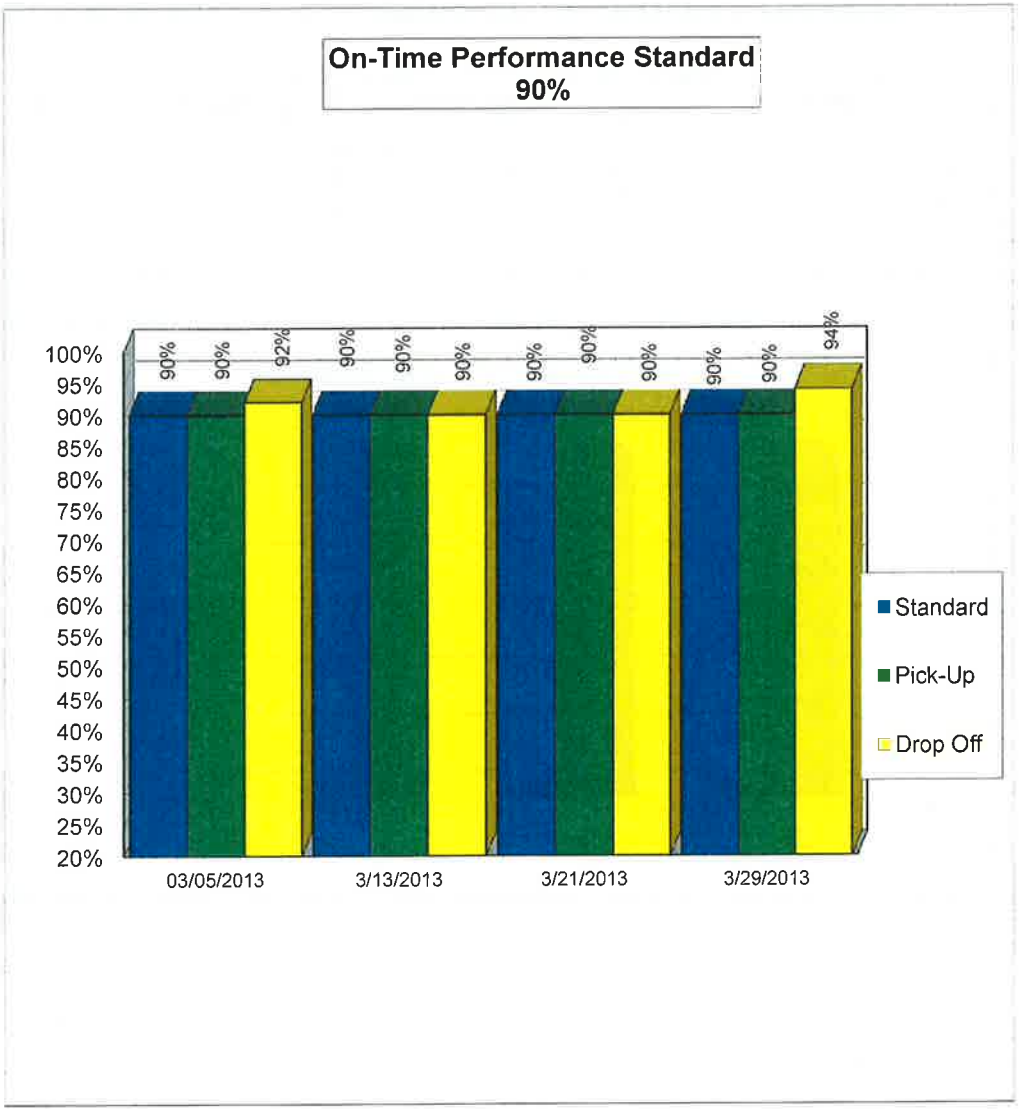
TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS ALACHUA COUNTY, JANUARY 2013



TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS ALACHUA COUNTY, FEBRUARY 2013



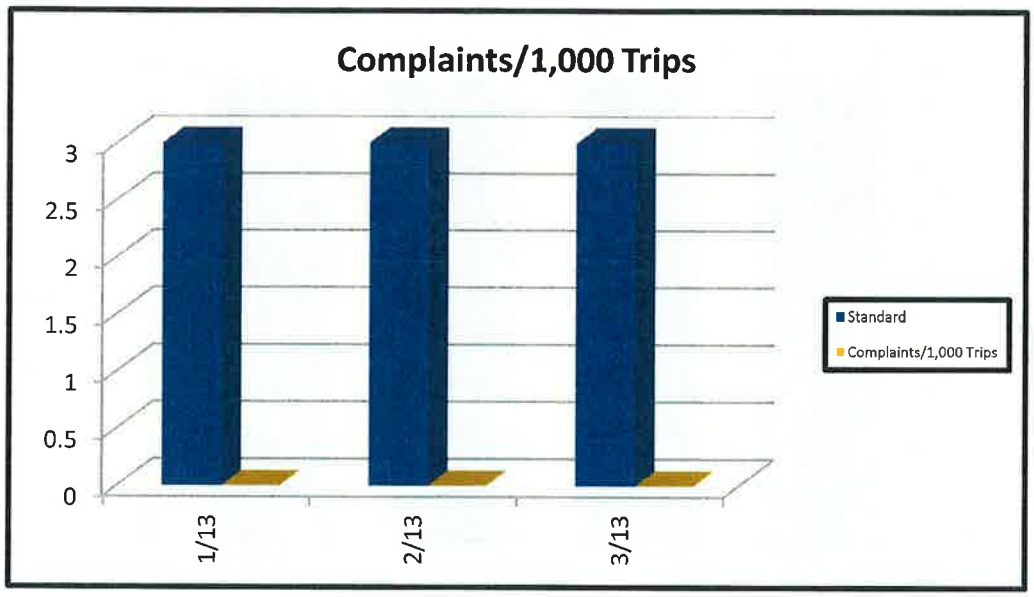
TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS ALACHUA COUNTY, MARCH 2013



TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS

ALACHUA COUNTY, JANUARY 2013 - MARCH 2013

MONTH	STANDARD	COMPLAINTS/1,000 TRIPS
1/13	3	0
2/13	3	0
3/13	3	0



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MV TRANSPORTATION
SUMMARY OF SERVICE ISSUES

TYPE OF COMPLAINT	7/12	8/12	9/12	10/12	11/12	12/12	1/13	2/13	3/13	4/13	5/13	6/13
Late Drop-Off	0	0	0	0	0	0	2	0	0	0		
Pick-Up before Window Opens	0	0	0	0	0	0	0	0	0	0		
Late Return Pick-Up	0	0	0	0	0	0	1	0	0	0		
Ride Time Exceeded Standards	0	0	0	0	0	0	0	0	0	0		
Can't Get Through by Telephone	0	0	0	0	0	0	0	0	0	0		
On Hold for Excessive Periods of Time	0	0	0	0	0	0	0	0	0	0		
Phone System Problems	0	0	0	0	0	0	0	0	0	0		
Sunday Reservations	0	0	0	0	0	0	0	0	0	0		
Trip Denial	0	0	0	0	0	0	0	0	0	0		
Driver Training	0	0	0	0	0	0	0	0	0	0		
Driver Behavior	0	0	0	0	0	1	0	0	0	0		
No Passenger Assistance Provided	0	0	0	0	0	0	0	0	0	0		
No Driver ID	0	0	0	0	0	0	0	0	0	0		
Dispatcher Behavior	0	0	0	0	0	0	0	0	0	0		
Reservationist Behavior	0	0	0	0	0	0	0	0	1	0		
Unsafe Driving	0	0	0	0	0	0	0	0	0	0		
No Show by Driver	0	0	0	0	1	0	0	0	0	0		
Reservations/Scheduling	0	0	0	0	0	0	0	0	0	0		
Reservations	0	0	0	0	0	0	0	0	0	0		
Air Conditioning not Working	0	0	0	0	0	0	0	0	0	0		
Wheelchair/Scooter Securement	0	0	0	0	0	0	0	0	0	0		
Passenger Behavior	0	0	0	0	0	0	0	0	0	0		
No Show by Passenger	0	0	0	0	0	0	0	0	0	0		
Customer Service	0	0	0	0	0	0	0	0	0	0		
Safety	0	0	0	0	0	0	0	0	0	0		
Trip Cancelled, Ride Came Anyway	0	0	0	0	0	0	0	0	0	0		
Wheelchair Lift Not Working Properly	0	0	0	0	0	0	0	0	0	0		
Charged Wrong Passenger Fare	0	0	0	0	0	0	0	0	0	0		
Vehicle Condition	0	0	0	0	0	0	0	0	0	0		
MV Staff Availability	0	0	0	0	0	0	0	0	0	0		
Dropped Off at Wrong Location	0	0	0	0	0	0	0	0	0	0		
Improper Passenger Assistance	0	0	0	0	0	0	0	0	0	0		
Did Not Process TD Eligibility Application	0	0	0	0	0	0	0	0	0	0		
Other	0	0	0	0	0	0	0	0	0	0		
TOTAL	**0**	**0**	**0**	**0**	**1**	**1**	**3**	**0**	**1**	**0**	**0**	**0**
TRIPS	**8,952**	**10,210**	**9,162**	**10,410**	**8,512**	**8,006**	**9,445**					
COMPLAINTS/1,000 TRIPS	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**
Number of Individuals Submitting Complaints	0	0	0	0	1	1	2	0	1			
RTS	0	0	0	0	0	0	0	0	0			
CIL	0	0	0	0	0	0	0	0	0			
Foster Grandparents	0	0	0	0	0	0	0	0	0			
NCFRPC	0	0	0	0	0	1	0	0	0			
COMMENDATIONS	**3**	**0**	**0**	**0**	**1**	**1**	**1**	**1**	**0**	**3**		

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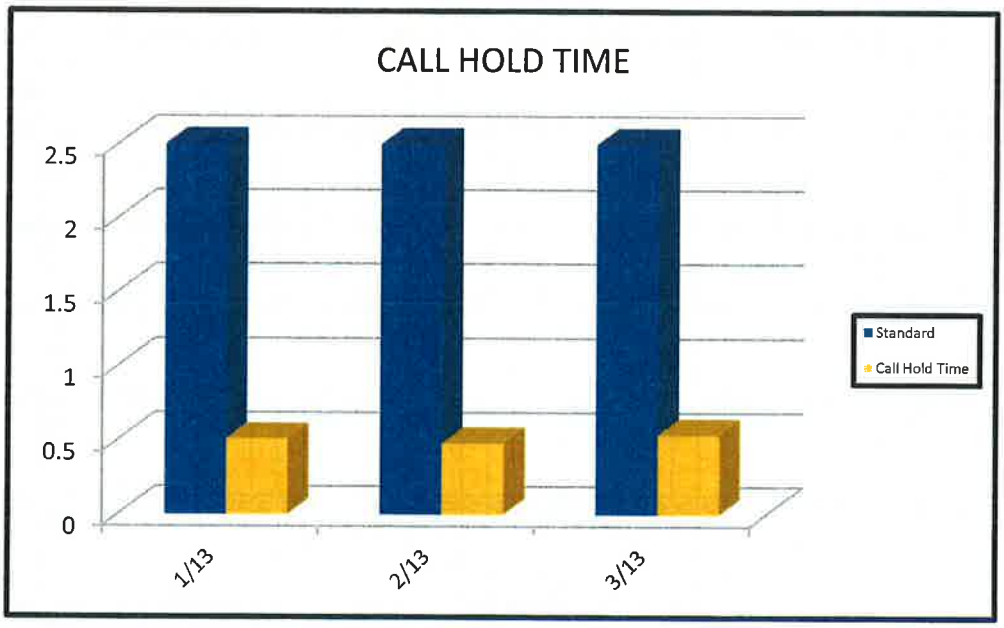
TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS ALACHUA COUNTY, JANUARY 2013 - MARCH 2013

MONTH	STANDARD	CALL HOLD TIME
1/13	2.5	0.51
2/13	2.5	0.48
3/13	2.5	0.54



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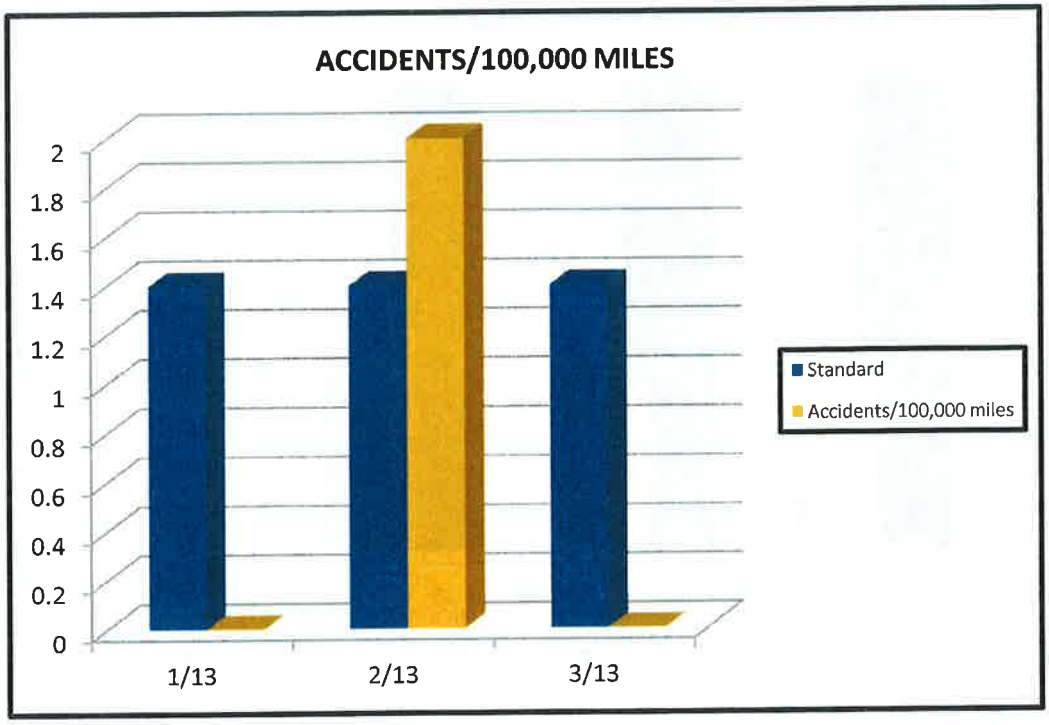
TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS

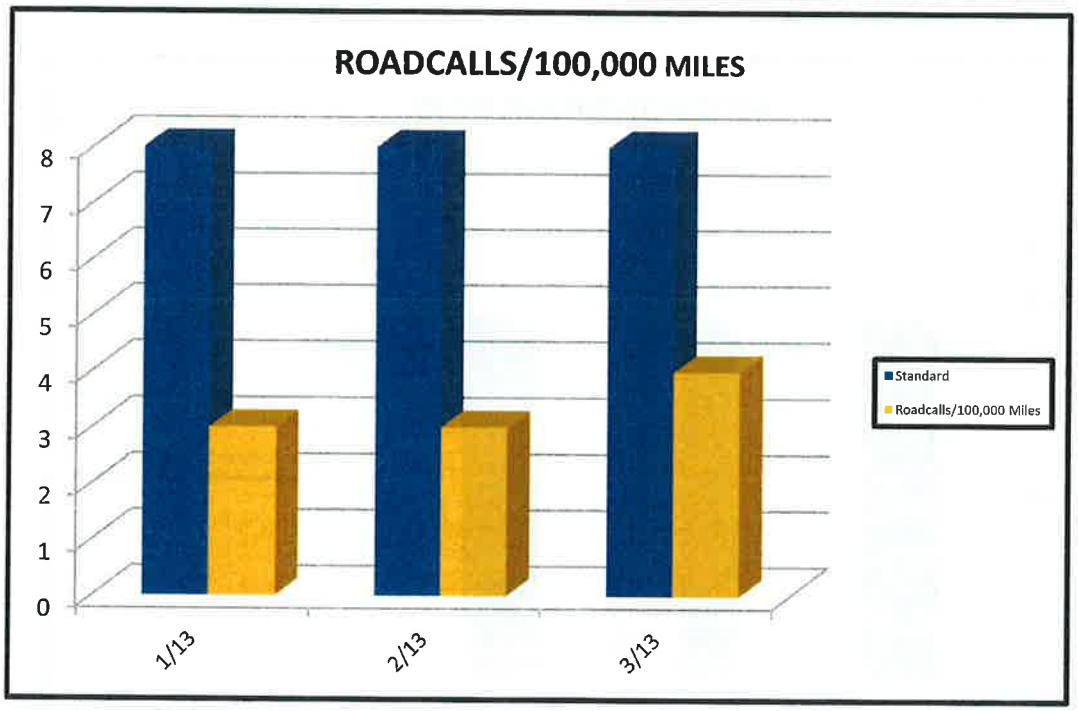
ALACHUA COUNTY JANUARY 2013 - MARCH 2013

MONTH	STANDARD	ACCIDENTS/100,000 MILES
1/13	1.4	0
2/13	1.4	2
3/13	1.4	0



TRANSPORTATION DISADVANTAGED SERVICE PLAN (TDSP) STANDARDS
ALACHUA COUNTY, JANUARY 2013 - MARCH 2013

MONTH	STANDARD	ROADCALLS/100,000 MILES
1/13	8	3
2/13	8	3
3/13	8	4



2012-2013 OPERATING DATA	Jul-12	Aug-12	Sep-12	Oct-12	Nov-12	Dec-12	Jan-13	Feb-13	Mar-13	Apr-13	May-13	Jun-13
Total No Trips Invoiced	**8,573**	**10,210**	**9,162**	**10,410**	**8,512**	**8,006**	**9,445**	**9,099**	**9,772**	**0**	**0**	**0**
Medicaid Alachua	3,232	3,439	2,907	3,257	2,831	2,907	3,154	2831	2,747			
TD Trust Fund Alachua	1,202	1,413	1,269	1,559	1,206	1,168	1,636	1401	1,647			
ADA	3,657	4,081	3,851	4,350	3,751	3,319	3,846	4004	4,484			
NFG - 5317	237	268	152	115	58	55	49	136	134			
CICO - 5311	63	66	76	160	170	159	149	132	138			
County of Alachua, FGPA, RSVP	137	519	558	537	445	340	558	511	536			
Elder Care	45	66	62	110	51	58	53	84	86			
Bus Passes TD	0	0	0	0	0	0	0	0	0			
Bus Passes Medicaid	0	0	0	0	0	0	0	0	0			
Purchased Transportation												
Medicaid Alachua	$102,394.00	$102,394.00	$102,394.00	$102,394.00	$102,394.00	$102,394.00	$102,394.00	$102,394.00	$102,394.00			
TD Trust Fund Alachua	$ 36,485.14	$ 45,975.81	$ 41,372.93	$ 52,347.43	$ 40,073.92	$ 37,993.10	$53,383.20	$45,522.35	$52,559.67			
ADA	$ 94,898.31	$ 105,985.82	$ 100,535.07	$ 116,396.51	$ 100,547.11	$ 88,507.92	$102,705.45	$106,966.51	$119,965.85			
NFG - 5317	$ 5,989.54	$ 6,781.62	$ 3,849.86	$ 3,088.69	$ 1,602.26	$ 1,468.67	$1,278.09	$3,535.94	$3,477.52			
CICO - 5311	$ 2,403.57	$ 1,733.76	$ 1,930.12	$ 3,958.74	$ 4,405.10	$ 4,259.09	$4,041.45	$3,581.88	$3,976.91			
County of Alachua, FGPA, RSVP	$ 4,041.50	$ 15,782.25	$ 17,045.97	$ 15,981.12	$ 13,243.20	$ 10,118.40	$ 16,606.08	$15,207.36	$15,951.36			
Elder Care	$ 1,482.75	$ 2,174.70	$ 2,042.90	$ 3,624.50	$ 1,680.45	$ 1,911.10	$ 1,746.35	$2,767.80	$2,833.70			
Bus Passes Total MED and TD	$ -	$ -	$ -	$ -	$ -	$ -	$ -	$ -	$ -			
Total Dollars Invoiced	**$ 251,748.71**	**$ 280,827.96**	**$ 269,170.85**	**$ 297,790.99**	**$ 263,946.04**	**$ 246,652.28**	**$282,154.62**	**$279,975.84**	**$301,159.01**			
Total Expenses	**$ 249,035.00**	**$ 262,239.00**	**$ 249,665.00**	**$ 259,049.00**	**$ 221,126.00**	**$ 232,894.00**	**$244,643.00**	**$ 224,564.00**	**$ 223,296.00**			
Average Cost Per Trip	**$ 29.05**	**$ 25.68**	**$ 27.25**	**$ 24.88**	**$ 25.98**	**$ 29.09**	**$ 25.90**	**$ 24.68**	**$ 22.85**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**
Total Vehicle Miles	**118,564**	**121,627**	**107,761**	**123,736**	**105,399**	**96,126**	**113,917**	**108,702**	**120,420**			
Total Vehicle Hours	**6,437**	**7,217**	**6,513**	**7,392**	**6,458**	**5,818**	**6,643**	**6,434**	**7,088**			
Avg Miles per Trip	**14**	**12**	**12**	**12**	**12**	**12**	**12**	**12**	**12**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**
Avg Cost Per Mile	**$2.10**	**$2.16**	**$2.32**	**$2.09**	**$2.10**	**$2.42**	**$2.15**	**$2.07**	**$1.85**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**
Avg Cost Per Hour	**$38.69**	**$36.34**	**$38.33**	**$35.04**	**$34.24**	**$40.03**	**$36.83**	**$34.90**	**$31.50**	**#DIV/0!**	**#DIV/0!**	**#DIV/0!**
Number of No Shows	**615**	**704**	**506**	**563**	**526**	**459**	**476**	**513**	**583**			
Number Trips Denied	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**	**0**			
No Accidents	**0**	**0**	**1**	**0**	**0**	**0**	**0**	**2**	**0**			
No RoadCalls	**2**	**5**	**2**	**6**	**8**	**3**	**3**	**3**	**4**			
Telephone Calls Rec'd	**9,424**	**10,927**	**9,274**	**10,808**	**8,408**	**8,093**	**12,471**	**12,184**	**12,088**			
Avg. Telephone On-Hold Time	**1.04**	**1.01**	**0.57**	**1**	**1**	**0.54**	**0.51**	**0.48**	**0.54**			

City Beautification Board

Gainesville, Florida

March 4, 2013

Mayor Craig Lowe
Gainesville City Commission
PO Box 490
Gainesville, FL 32627-0490

Dear Mayor Lowe,

As an advisory board to the Gainesville City Commission, the City Beautification Board (CBB) strives for excellence in civic design conducting studies and making recommendations to the City Commission on roadways and landscaping, and in this case, on mast-arm traffic signals. Concerning the mast-arm traffic signals, in the 1990s, the CBB recommended and it was approved by the Commission and original Metropolitan Transportation Planning Organization (MTPO), that going forward all installations would keep a uniform design and color according to these specifications. Hence, the CBB strongly urges the continued installation of black mast-arm traffic signals in the Gainesville urban area.

When the MTPO opted for mast-arm traffic signals with horizontally mounted signal heads more than fifteen years ago, the MTPO also specified to the Florida Department of Transportation (FDOT) that all new fixtures be painted ebony black to match other street hardware. As a result, we have seen major improvements in the appearance of our streets and gateway corridors.

Now, apparently, FDOT has indicated that it can no longer pay for the cost of painting mast-arm signals black to match the street lights, bus stops, bike racks, refuse containers, and other street hardware. As a result, the new mast-arm traffic signals at 5700 Northwest 34th Street adjacent to the new Super Wal-Mart were not painted black according to original MTPO specifications. This is a glaring example of what signals look like when they do not match other street hardware. In fact, at this intersection, the mast-arm signals are unpainted while pedestrian traffic signals are black, creating an unnecessary hodge-podge of street hardware.

Perhaps Wal-Mart could have covered the cost of painting these mast-arm signals, if it had been asked, and perhaps could still do so.

As the City and County move forward on the construction and renovation of many streets and gateway corridors, this is an important design feature that needs to be addressed and funded. Installing additional unpainted mast-arm signals will be visually inconsistent with existing hardware and create an unattractive combination of painted and unpainted signals.

We hope the City Commission and MTPO will be able to provide the resources needed to implement "best design practices" on our streets and gateway corridors in future, and stick to the agreed upon civic design for Gainesville's streets.

Sincerely,

Anita Spring, Chair
City Beautification Board

XC: Gainesville City Commission
Metropolitan Transportation Planning Organization Chair Randy Wells
Metropolitan Transportation Planning Organization Vice-Chair Susan Baird
Russ Blackburn, City Manager
Paul Folkers, Assistant City Manager
Steven R. Phillips, Director, Parks, Recreation and Cultural Affairs
City Beautification Board Staff Liaison Earline Luhrman, Urban Forestry Inspector
City Beautification Board Members

Florida Department of Transportation

RICK SCOTT
GOVERNOR

2198 Edison Avenue
Jacksonville, FL 32204-2730

ANANTH PRASAD, P.E.
SECRETARY

Transmitted electronically to: byerly@alachuacounty.us ; mayor@cityofgainesville.org; sanderson@ncfrpc.org

March 18, 2013

The Honorable Mike Byerly, Chair
Alachua County Board of County Commissioners
12 SE 1st Street
Gainesville, FL 32601

The Honorable Craig Lowe, Mayor
200 E. University Ave.
Gainesville, FL 32601

Subject: SR 329 (Main Street) transfer from Depot Avenue to SR 331 (Williston Road)

Dear Commissioner Byerly and Mayor Lowe:

The December 3, 2012, meeting of the Gainesville MTPO included a presentation by the Gainesville Community Redevelopment Agency (CRA) regarding Main Street south of Depot Avenue. The presentation outlined changes to the roadway typical section including reducing the number of travel lanes, on-street parking, medians, etc. As explained at the meeting, Main Street remains under the jurisdiction of the Florida Department of Transportation as SR 329 between SR 331 (Williston Road) and Depot Avenue. Prior commitments by the Alachua County Board of County Commissioners included the transfer of this section of Roadway from the Department to Alachua County upon the completion of the reconstruction of Main Street between Depot Avenue and NW 8th Avenue. The Department has completed the reconstruction project. However, the transfer of Main Street to Alachua County has not been completed due to changes by the Alachua County Board of County Commissioners.

The Florida Department of Transportation encourages the Alachua County Board of County Commissioners and the City of Gainesville to work together to reach an agreement on which agency should assume the ownership and maintenance of Main Street. Until such time that the above reference section of roadway is removed from the state system, no further modifications to Main Street will be approved by the Department. Any requested modifications or changes to a state facility must be submitted to the Department and a permit issued in advance of any activities with the Department's rights-of-way.

The Department welcomes the opportunity to work with Alachua County and the City of Gainesville to finalize the ownership and maintenance responsibilities for this section of roadway. If you have any questions or need any further information, please contact me at (904) 360-5646 or via email at James.Bennett@dot.state.fl.us.

Respectfully,

James G. Bennett, P.E.
Urban Transportation Development Manager

CC: Alachua County Commissioners
City of Gainesville Commissioners
Gainesville MTPO

CA.12

Funding Florida's Future Transportation System:

MPOAC Transportation Revenue Study

605 Suwannee Street • MS 28B • Tallahassee, FL 32399-0450
(850) 414-4037 • (850) 414-4895 fax
www.mpoac.org

MPOAC

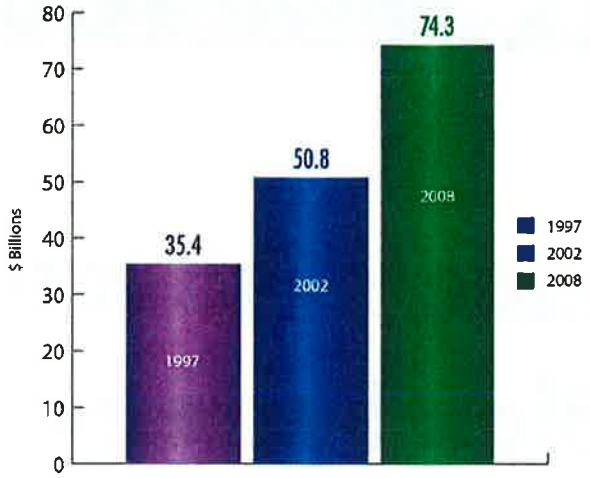
THE ISSUE: The Gas Tax is a Declining Source of Funding for Meeting our Transportation Needs

- Good transportation systems are closely linked with economic health. When transportation systems are efficient, they provide accessibility to markets, employment and additional investments. When they are deficient, they can have economic costs, such as reduced or missed opportunities.

- Most of the money to pay for transportation projects in Florida comes from the gas tax. But the gas tax is a declining and unsustainable source of transportation funding and we are falling behind in meeting our transportation needs.

- In 2008, the MPOAC estimated that the shortfall between the cost of needed transportation projects and reasonably expected revenues in Florida's metropolitan areas to be more than $74 billion, more than doubling the estimated statewide shortfall in under a decade.

Florida Metro Area Funding Shortfall Estimates

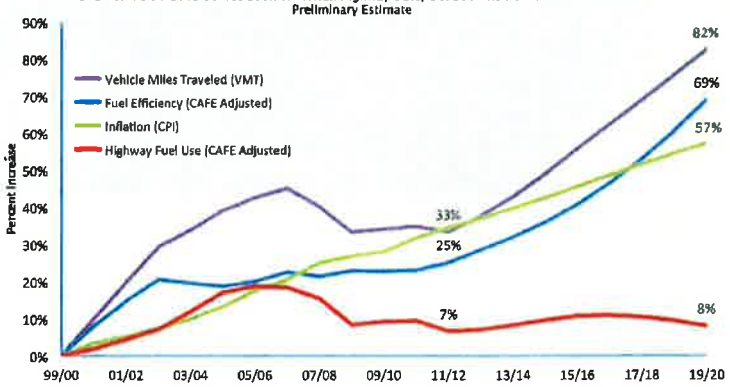


- In fact, our transportation needs are continuing to grow in absolute terms as vehicle miles of travel (VMT) on our roads is expected to grow substantially between now and the year 2020. Meanwhile, vehicles continue to get better gas mileage with the fleet becoming 69% more efficient by the year 2020 as compared to 2000 due to new Corporate Average Fuel Efficiency standards. These factors, combined with inflation eroding other transportation revenue sources, will result in a flattening of fuel use and a stagnation in transportation purchasing power.

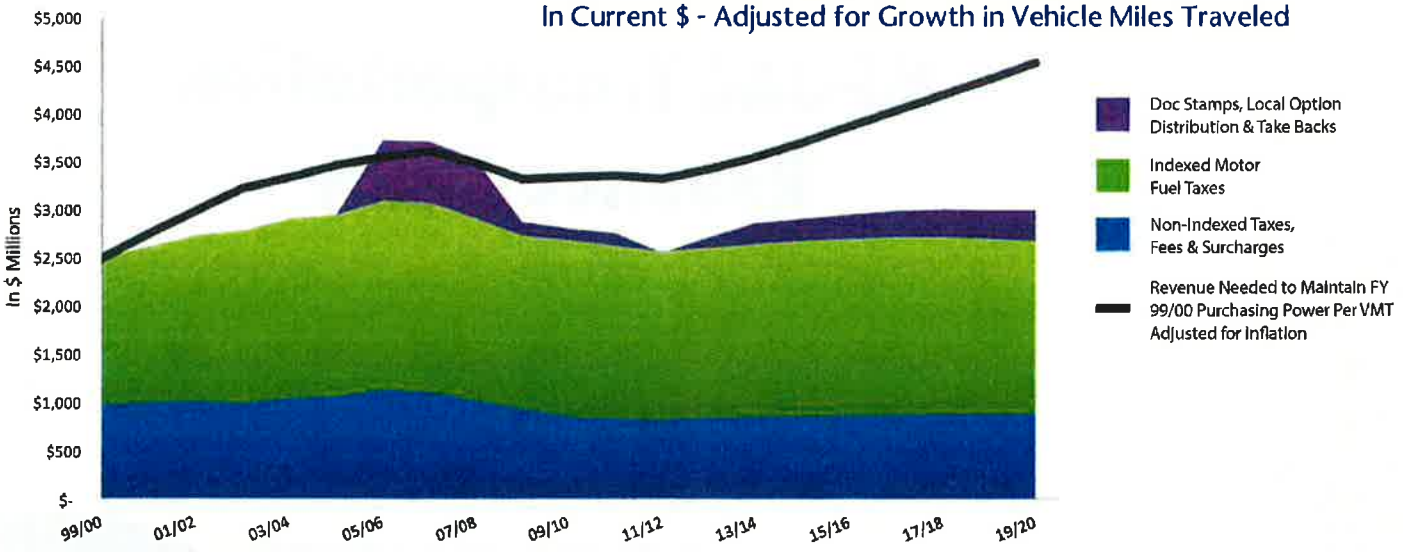
State Transportation Trust Fund Purchasing Power

Variables Affecting Fuel Consumption and Transportation Revenues

Sources: FDOT 2009 Source Book of Florida Highway Data, Oct 2011 REC & 2012-2025 CAFE Preliminary Estimate



State Transportation Revenues
$12.1 Billion in Lost Purchasing Power
In Current $ - Adjusted for Growth in Vehicle Miles Traveled



The bottom line is Florida needs a sustainable transportation funding source for the future.

THE GOAL

The Florida MPO Advisory Council (MPOAC), in cooperation with the Center for Urban Transportation Research (CUTR) at the University of South Florida, completed a 2 year study on the level of funding for transportation investments in Florida. Recognizing that current funding sources are declining and unsustainable over the long term, the MPOAC developed recommendations to deal with the future transportation funding needs of our metropolitan areas and state.

The transportation revenue study was guided by an advisory committee made up of diverse transportation interests representing business organizations, local governments, environmental groups and freight shippers. The MPOAC set a goal of ensuring that the purchasing power of today's transportation dollar is equivalent to what it could purchase in the year 2000.

THE RECOMMENDATION: Identifying Strategies to Meet our Transportation Needs

The MPOAC developed a set of options that are sustainable, innovative and that deal with the on-going transportation funding needs of our state.

Top six options that were considered most feasible...

	Revenue Option	8 year Total ($millions)	Annual Average ($millions)
1	1 Cent Municipal Optional Sales Tax - *Local*	6,637	830
2	Index All Fuel Taxes not Currently Indexed - *Local*	918	115
3	Return Motor Vehicle License, Initial Vehicle Registration and Titling Fee Increases to the State Transportation Trust Fund - *State*	3, 301	413
4	2 Cent Fuel Tax Increase per Year for 5 Years (10 cents) Indexed - *State*	6,424	803
5	5 Cent Diesel Tax - *Local*	576	72
6	Mileage Based User Fee Study	-	-

INTERIM RECOMMENDATIONS: Filling the Revenue Gap

The MPOAC identified 5 solutions the legislature could enact to raise $12.1 billion and restore the purchasing power of our transportation dollar to what it could purchase in 2000

- Expand charter county surtax to allow all counties in MPO areas (adds 12 counties) and municipalities over 150,000 in population to go to local referendum for up to a 1% sales tax
- Index local option fuel taxes in a manner similar to how state fuel taxes are indexed
- Return tag and title fee increase to STTF that were enacted by the State in 2009
- Increase the state gas tax by 2 cents per year for the next 5 years
- Increase diesel fuel tax by 5 cents with proceeds directed to commercial freight and trucking related projects

LONG TERM RECOMMENDATION: Making the Revenue Source Sustainable

Develop a plan to implement a Mileage Based User Fee in Florida that does not rely on GPS technology as a tracking device.

THE MPOAC

The Metropolitan Planning Organization Advisory Council (MPOAC) represents the collective interests of Florida's 26 MPOs, and assists the MPOs in carrying out the urbanized area transportation planning process by serving as the principal forum for collective policy discussion. The MPOAC was created by the Florida Legislature pursuant to Section 339.175, Florida Statutes, to augment and not supplant the role of the individual MPOs in the cooperative transportation planning process.

MPOAC Transportation Revenue Study Options

Top Options

	Revenue Option	8 year Total ($millions)	Annual Average ($millions)
1	1 Cent Municipal Optional Sales Tax - *Local*	6,637	830
2	Index All Fuel Taxes not Currently Indexed - *Local*	918	115
3	Return Motor Vehicle License, Initial Vehicle Registration and Titling Fee Increases to the State Transportation Trust Fund - *State*	3, 301	413
4	2 Cent Fuel Tax Increase per Year for 5 Years (10 cents) Indexed - *State*	6,424	803
5	5 Cent Diesel Tax - *Local*	576	72
6	Mileage Based User Fee Study	-	-
7	State Sales Tax @ 6% in Lieu of Fuel Taxes, with a floor - *State*	1,087	136
8	Create an Independent Toll Rate Setting Commission - *State*	-	-
9	Regional Trans Financing Authority @ $100 million per year	3,200	400
10	Sales Tax on Motor Vehicle Parts & Services	5,331	666
11	Redirect Sales Tax on Battery Electric Vehicles to State Transportation Trust Fund	73	9
12	County $10 Registration Fee - *Local*	1,242	155
13	Alternative Fuel Decal Expansion - *State*	204	26
14	$100 million in New Toll Projects	2,450	306

For more information, please go to http://www.mpoac.org/revenuestudy/

Meeting

Agenda

Enclosures

North Central Florida Regional Planning Council

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2009 NW 67th Place, Gainesville, FL 32653-1603 • 352.955.2200

May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Citizens Advisory Committee Vacant Position

MTPO STAFF RECOMMENDATION

Fill one vacant position on the Citizens Advisory Committee for a term expiring December 2013.

BACKGROUND

The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area needs to fill one vacant position on the Citizens Advisory Committee. There are six candidates for the vacant position.

> *Note- According to Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area rules, "no more than 25 percent of the membership of the Citizens Advisory Committee shall be composed of individuals residing outside the Gainesville Urbanized Area." Two of the applicants reside outside the Gainesville Urbanized Area boundary and either one may be appointed to the Citizens Advisory Committee.*

ENCLOSED MATERIALS

Enclosed please find:

1. Exhibit I which gives the names and occupations of current CAC members and applicants;
2. Exhibit II which shows where current CAC members and applicants live;
3. Exhibit III which is a blank ballot to assist in the voting process; and
4. the current pool of applications.

CITIZENS ADVISORY COMMITTEE MEMBERSHIP - FLORIDA STATUTES

339.175(5)6 (e)1 "Each M.P.O. [Metropolitan Planning Organization] shall appoint a citizens' advisory committee, the members of which serve at the pleasure of the M.P.O. [Metropolitan Planning Organization]. The membership on the citizens' advisory committee must reflect a broad cross section of local residents with an interest in the development of an efficient, safe, and cost-effective transportation system. Minorities, the elderly, and the handicapped must be adequately represented."

CITIZENS ADVISORY COMMITTEE MEMBERSHIP RULES

1.041 (1)- The technical personnel directly shall be composed of no less than ten (10) persons and no more than fifteen (15). No more than twenty-five percent (25%) of the membership of the Citizens Advisory Committee shall be composed of individuals residing outside the Gainesville Urbanized Area.

Currently, there is one member of the CAC that lives outside the Gainesville Urbanized Area Three of the applicants live outside of the Gainesville Urbanized Area.

1.041 (2)- Voting members of the Citizens Advisory Committee shall not be elected officials or technical personnel directly involved in the transportation planning of the Gainesville Urbanized Area.

1.041 (3)- The Citizens Advisory Committee shall be composed of citizens from the Gainesville Urbanized Area and Alachua County selected to provide a broad cross-section of citizens with an interest in the development of an efficient, safe and cost effective transportation system.

Note: There is no policy regarding how many times a member may serve on the Committee.

VOTING PROCEDURE

The voting policy that was approved in October, 2001 is to always appoint Advisory Committee members by roll-call vote by voice and, when there are several vacant positions to fill, also fill out ballots and submit them to staff for tabulation.

EXHIBIT I

CURRENT CITIZENS ADVISORY COMMITTEE (CAC)
TERMS OF APPOINTMENT

CURRENT MEMBERS~	OCCUPATION	TENURE (In Years)	TERM EXPIRES
Nelle Bullock	Social Worker	5.0	December, 2013
John Richter	Residential Designer	2.5	December, 2013
Holly Shema	Student	3.0	December, 2013
Ewen Thomson	Business Owner/Consultant	7.5	December, 2013
VACANT	-		December, 2013
E. J. Bolduc III	Landscape Architect	1.5	December, 2014
Rob Brinkman	Carpenter	7.5	December, 2014
Mary Ann DeMatas	Hotel Clerk	3.5	December, 2014
James Samec	Retired Police Office	5.5	December, 2014
Ruth Steiner	University of Florida Professor	11.5	December, 2013
Thomas Bolduc	Pharmacy Operations Manager	0.5	December, 2015
Rajeeb Das	Analyst	0.5	December, 2015
Jan Frentzen	General Contractor	16.5	December, 2015
Melinda J. Koken	Retired General Contractor	1.5	December, 2015
Chandler Otis	Bicycle Mechanic, Sales Clerk	7.5	December, 2015
APPLICANTS*#~	**OCCUPATION**	-	-
Gwen Creel~	Physical Therapist	-	-
Luis Diaz	Town of Tioga Director of Development	-	-
Blake Fletcher#	Real Estate Agent and Mortgage Broker	4.0	-
Gilbert Levy	Town of Tioga President	-	-
Jean Rochford	Retired	-	-
Chris Towne#~	Civil Engineer	2.0	-

~ Member or Applicant Living Outside MTPO Boundary
* Current Member Seeking Reappointment
# Former Member Seeking Appointment

EXHIBIT II

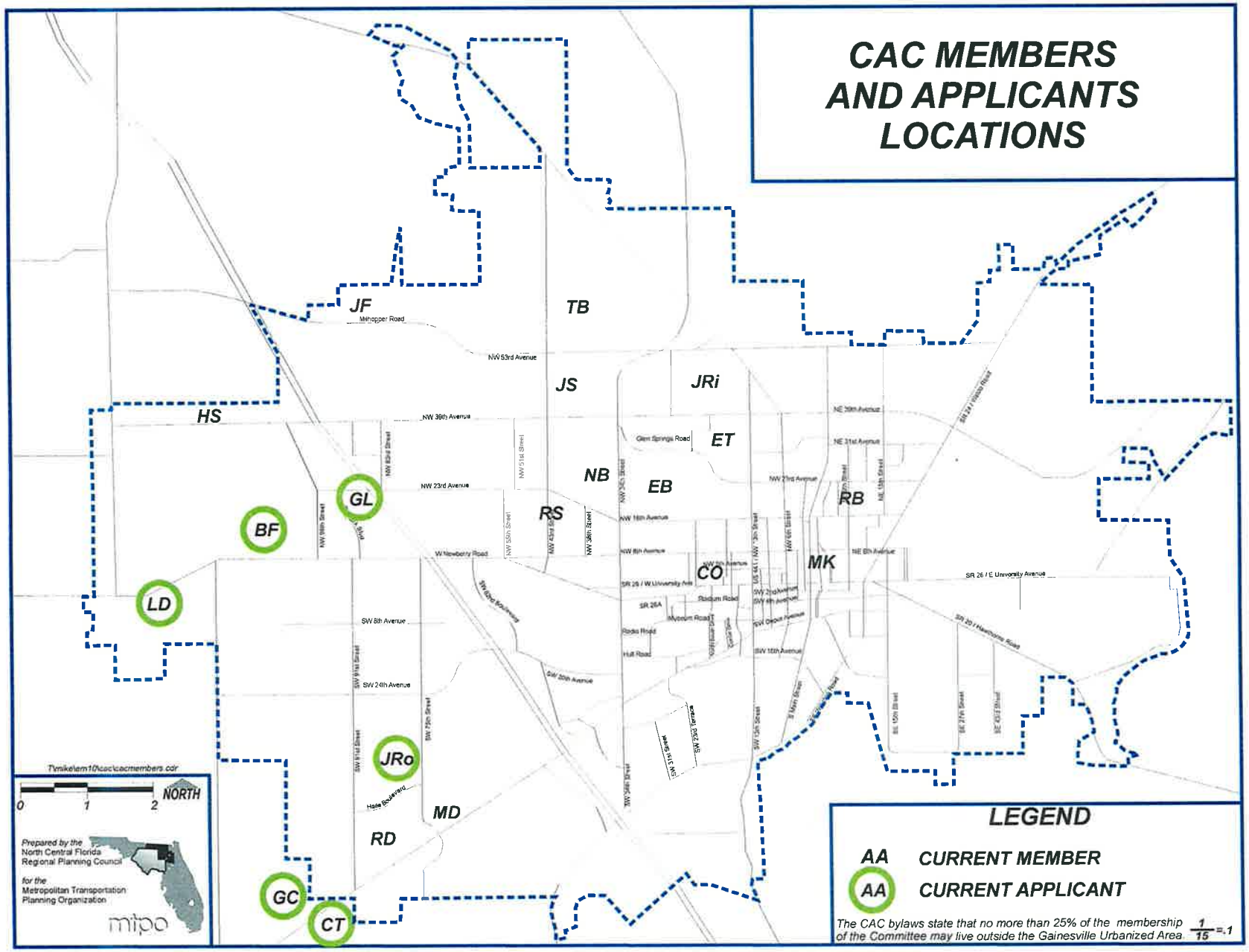
CAC MEMBERS AND APPLICANTS LOCATIONS
JF
TB
JS
JRi
HS
ET
NB
EB
RB
GL
BF
RS
CO
MK
LD
JRo
MD
RD
GC
CT
NW 53rd Avenue
NW 39th Avenue
NW 23rd Avenue
W Newberry Road
SW 8th Avenue
SW 24th Avenue
Hale Boulevard
SR 26 / E University Avenue
Tmikelem10\cac\cacmembers.cdr
0
1
2
NORTH
Prepared by the North Central Florida Regional Planning Council
for the Metropolitan Transportation Planning Organization
mtpo
LEGEND
AA CURRENT MEMBER
AA CURRENT APPLICANT
The CAC bylaws state that no more than 25% of the membership of the Committee may live outside the Gainesville Urbanized Area. 1/15 =.1

EXHIBIT III

MTPO MEMBER	*Gwen CREEL*	Luis DIAZ	Blake FLETCHER	Gilbert LEVY	Jean ROCHFORD	*Chris TOWNE*
Susan BAIRD						
Susan BOTTCHER						
Mayor Ed BRADDY						
Mike BYERLY						
Todd CHASE						
Charles CHESTNUT IV						
Thomas HAWKINS						
Yvonne HINSON-RAWLS						
Robert HUTCHINSON						
Lee PINKOSON						
Lauren POE						
Randy WELLS						

Fill one [1] vacant position for a term ending in 2013.
Applicants shown in *italics* and shaded column reside outside the MTPO Boundary.

t:\marlie\ms13\mtpo\memo\cacmbrappt_jun3x3.docx

GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to: Date 9/21/12

MTPO
North Central Florida Regional Planning Council
2009 NW 67th Place - Suite A
Gainesville, FL 32653-1603

NAME Gwen Creel ADDRESS 10127 SW 61 Ave Gainesville

ZIP CODE 32608 AGE 54 TELEPHONE (H) 352-335-9189

EMAIL gcreel@phhp.ufl.edu (W) ~~3~~ cell - 352-262-9189

HOW LONG A RESIDENT OF ALACHUA COUNTY? 25 years
ARE YOU CURRENTLY AN ELECTED OFFICIAL? No

OCCUPATION physical therapist

EDUCATION BHS, MHS

TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT
recreational bicyclist

PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS None

I am willing to spend 10 hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations.
Additional information on me may be secured from: (List three references - name, address, and phone number.)

Don Davis 3509 NW 53rd Terr. Gainesville, FL 32606 352-373-3211
Chandler Otis 2123 NW 4th Place Gainesville, FL 32603 352-376-4963
Roger Pierce 5015 NW 19th Place Gainesville, FL 32605 352-378-7063

Gwen Creel
Signature

Additional information may be attached to this form.

NORTH CENTRAL FLORIDA
RECEIVED
SEP 21 2012
REGIONAL PLANNING COUNCIL

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GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to:

Date: 2/15/13

MTPO
North Central Florida Regional Planning Council
2009 NW 67 Place, Suite A
Gainesville, FL 32653-1603
FAX: 352/955-2209

NAME Luis A. Diaz ADDRESS 104 SW 131st Street, Tioga, FL
ZIP CODE 32669 AGE 50 TELEPHONE (H) (352) 331-6220
EMAIL diaz@townoftioga.com (W) (352) 258-9783
FAX: (352) 331-7452
HOW LONG A RESIDENT OF ALACHUA COUNTY? 35 years
ARE YOU CURRENTLY AN ELECTED OFFICIAL? NO
OCCUPATION Director of Development for Town of Tioga
EDUCATION Bachelor Degree - Building Construction - UF - 1986
TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT:
Florida Green Build Coalition Certified Agent. Currently on Board for the Builders Association of NCF, State of FL Builders Assoc., Gainesville Community Foundation, City of Gainesville Community Development Re. Committee. & Haven Hospice Community Advisory Committee
PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS 4½ yrs City of Gainesville Historic Preservation Board, past Chairman for Reichert House & Currently serving on Board for Cade Museum.

I am willing to spend 10 hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations. Additional information on me may be secured from: (List three references - name, address, and phone number)

Steve Lachnicht - 104 SW 2nd Avenue, Gainesville, FL 32601 (352) 374-5249
Jay Brown - 3530 NW 43rd Street, Gainesville, FL 32606 (352) 375-5999
Rick Drummond - 12 SE 1st Street, Gainesville, FL 32601 (352) 374-5204

Signature

Additional information may be attached to this form.

NORTH CENTRAL FLORIDA
RECEIVED
FEB 15 2013
REGIONAL PLANNING COUNCIL

T:\Mb-Irm\Bl\rcd\CAC-application.wpd January 7, 2003

GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to:

Date: 12/2/09

MTPO
North Central Florida Regional Planning Council
2009 NW 67 Place, Suite A
Gainesville, FL 32653-1603
FAX: 352/955-2209

NAME Blake Fletcher ADDRESS 11514 NW 15th Ln Gainesville
ZIP CODE 32606 AGE 27 TELEPHONE (H) _____
EMAIL BlakeFletcher@Bosshardtrealty.com (W) 374-4007
FAX: 378-2737
HOW LONG A RESIDENT OF ALACHUA COUNTY? 27 yrs
ARE YOU CURRENTLY AN ELECTED OFFICIAL? NO
OCCUPATION Real Estate
EDUCATION 4 yr degree from Millsaps College
TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT:
As a real estate professional I'm well versed in Transportation and planning issues.

PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS MTPO CAC
Alzheimer's Association.

I am willing to spend As many as needed hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations. Additional information on me may be secured from: (List three references - name, address, and phone number)

Blake Fletcher
Signature

Additional information may be attached to this form.

NORTH CENTRAL FLORIDA
RECEIVED
DEC 03 2009
REGIONAL PLANNING COUNCIL

T:\Mike\cm08\cac\CAC-application.wpd January 7, 2003

GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to:

MTPO
North Central Florida Regional Planning Council
2009 NW 67 Place, Suite A
Gainesville, FL 32653-1603
FAX: 352/955-2209

Date: March 1, 2013

NAME Gilbert A. Levy, Jr. ADDRESS 7719 NW 18th Lane, Gainesville

ZIP CODE 32605 AGE 54 TELEPHONE (H) 352-373-7637

EMAIL gil-levy@msn.com (W) 352-333-2580

FAX: 352-331-4201

HOW LONG A RESIDENT OF ALACHUA COUNTY? 31 years

ARE YOU CURRENTLY AN ELECTED OFFICIAL? NO

OCCUPATION Asset Manager and President Tioga Town Center

EDUCATION Bachelor and Graduate degrees in Banking and Finance

TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT :

Developer, Landlord - Tioga Town Center and Jonesville Plaza - 8 years

PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS

Member USSBA Advisory Council, North Florida District, Board Member Newberry-Jonesville Chamber of Commerce, Board Member YMCA, Board Member SF Foundation,

I am willing to spend 35-40 hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations. Additional information on me may be secured from: (List three references - name, address, and phone number)

Laude Arnaldi - Commercial Lender, Florida Credit Union - 352-377-4141

A. J. Brown - Brown and Cullen - 352-375-8999

Jack Ponikvar - Ponikvar and Associates - 352-372-1378

Signature

Additional information may be attached to this form.

T:\Mike\em08\cac\CAC-application.wpd January 7, 2003

VIA FAX 4
USPS

GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to:

MTPO
North Central Florida Regional Planning Council
2009 NW 67 Place, Suite A
Gainesville, FL 32653-1603
FAX: 352/955-2209

Date: Nov. 14, 2012

NAME Jeanne Rochford ADDRESS 8647 SW 42nd Pl. Gainesville,
ZIP CODE 32608 AGE 79 TELEPHONE (H) 352-335-9503
EMAIL jeannerochford@gmail.com (W) 352-262-8524
FAX: none
HOW LONG A RESIDENT OF ALACHUA COUNTY? 22 years
ARE YOU CURRENTLY AN ELECTED OFFICIAL? No
OCCUPATION Retired
EDUCATION BA-Communications - University of Illinois
TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT :
Executive Director - Keep Alachua County Beautiful
Worked w/ state & federal transportation

PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS Keep Alachua County Beautiful recognized nationally
1998 Environmentalist - Gville Sun, 1st/best Executive Director in FL.

I am willing to spend 1-4 hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations. Additional information on me may be secured from: (List three references - name, address, and phone number)

Father Jeff McCowan Queen of Peace 16900 SW 24th Ave Gville 32608 352-332-8808
Bob Gachsé 1111 NW 25th Terr. Gainesville, FL 32605 352-376-0402
Jim Brand 3135 SE 27th St. Gainesville, FL 352-372-2528

Jeanne Rochford
Signature

Additional information may be attached to this form.

T:\Mike\em08\cac\CAC-application.wpd January 7, 2003

GAINESVILLE URBANIZED AREA
METROPOLITAN TRANSPORTATION PLANNING ORGANIZATION (MTPO)

CITIZENS ADVISORY COMMITTEE

RESUME

Please return to:

MTPO
North Central Florida Regional Planning Council
2009 NW 67 Place, Suite A
Gainesville, FL 32653-1603
FAX: 352/955-2209

Date: 11/5/2010

NAME CHRIS TOWNE, PE ADDRESS 10180 SW 104TH AVE, GAINESVILLE
ZIP CODE 32608 AGE 37 TELEPHONE (H) 352-641-6096
EMAIL ctowne@dimp.com (W) 352-371-2741
FAX: 352-372-4318
HOW LONG A RESIDENT OF ALACHUA COUNTY? 3+ YEARS
ARE YOU CURRENTLY AN ELECTED OFFICIAL? NO
OCCUPATION CIVIL ENGINEER
EDUCATION BSCE VIRGINIA TECH 1996
TRAINING OR EXPERIENCE RELATED TO ACTIVITIES FOR THIS APPOINTMENT :

14 YEARS EXPERIENCE IN CIVIL ENGINEERING WITH APPROXIMATELY 7 OF THOSE YEARS WORKING ON TRASPORTATION PROJECTS.
ACTIVE MEMBER OF AMERICAN SOCIETY OF CIVIL ENGINEERS

PAST CIVIC AND PROFESSIONAL ACCOMPLISHMENTS OR HONORS
PROFESSIONAL ENGINEER (WV 2000)(FL 2007)

I am willing to spend 8 hours per month on this activity. I will attend meetings in accordance with the adopted policies of the MTPO. If at any time my business or professional interests conflict with the interests of this board or committee, I will not participate in such deliberations. Additional information on me may be secured from: (List three references - name, address, and phone number)

JOE MONTALTO, PE 309 SE 7TH ST, GAINESVILLE 32601 352-494-6225
CHRIS COLEMAN, IAR 10312 NW 13TH LN, GAINESVILLE 32606 352-332-6595
WESLEY EMMANUEL 1806 NW 22ND DR, GAINESVILLE 32605 352-371-1417

Signature

Additional information may be attached to this form.

T:\Mike\em08\cac\CAC-application.wpd January 7, 2003

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Certification of the Metropolitan Transportation Planning Process

STAFF RECOMMENDATION

Authorize the Chair to sign the Joint Certification Statement (see Exhibit 2).

BACKGROUND

Federal law and regulation requires the Florida Department of Transportation and the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area to jointly certify each year the transportation planning process, concurrent with the submittal of the transportation improvement program.

A joint review meeting with the Florida Department of Transportation was held on May 10, 2013. In the attached Exhibit 1 memorandum, the Florida Department of Transportation states that no corrective actions have been identified and it has certified the metropolitan transportation planning process. Exhibit 2 is the Joint Certification Statement that needs to be signed and returned to the Florida Department of Transportation.

The reason that this is not on the Consent Agenda for action is to address the final step in this certification process which is to provide citizens the opportunity to comment on the transportation planning process.

t:\marlie\ms13\mtpo\memo\certifyjune3.docx

EXHIBIT 1

Florida Department of Transportation
2198 Edison Avenue - MS 2812
Jacksonville, FL 32204

TO: Marlie Sanderson, AICP
Director of Transportation Planning

FROM: Karen Taulbee, AICP
Florida Department of Transportation

DATE: May 17, 2013

Subject: 2013 Joint Certification Process
Metropolitan Transportation Planning Organization (MTPO) for the Gainesville Urbanized Area

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In accordance with Chapter 7 of the MPO Program Management Handbook and cited Federal regulations, the Metropolitan Transportation Planning Organization (MTPO) and the Florida Department of Transportation (FDOT) must jointly certify the metropolitan transportation planning process. The FDOT and MTPO staff held a kick off meeting to discuss the certification process on January 23, 2010. Our joint certification review meeting was held in Jacksonville on May 10, 2013.

For the past three (3) years, the MTPO and FDOT elected to do a modified certification review. This year, 2013, the MTPO and FDOT conducted a standard review encompassing the ten (10) areas of law specified in 23 C.F.R. 450.334. Although the MTPO is not a Transportation Management Area (TMA) the standard review for the MTPO was more in depth than the modified review.

The MTPO staff did an excellent job providing in-depth discussion, documentation and examples for all of the areas covered. The reflection of the MTPO's public involvement in all aspects of the planning process is very evident with the in-depth review.

The FDOT review did not identify any corrective actions.

The FDOT review did provide the following recommendations to the MTPO:

- The MTPO has submitted the Draft Planning Area Boundary maps and has submitted re-affirmation of the apportionment plan to the Executive Office of the Governor. The FDOT recommends reviewing all agreements that are pertinent to the establishment and operation of the MTPO to ensure they are current.
- FDOT staff recommends the MTPO continue with the update of the MTPO and Committee Bylaws and finalize the review and adoption.

Florida Department of Transportation certifies the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area transportation planning process.

Please provide the final Certification Package including the signed Joint Certification Statement to the FDOT liaison for final transmittal. Should you have any questions or need additional information, please contact me at Karen.Taulbee@dot.state.fl.us or 904-360-5652.

EXHIBIT 2

Joint Certification Statement on the Metropolitan Transportation Planning Process

Pursuant to the requirements of 23 United States Code 134 (k)(5), 23 Code of Federal Regulations 450.334(a), the Department and the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area have performed a review of the certification status of the metropolitan transportation planning process for the **Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area** with respect to the requirements of:

1. 23 United States Code. 134 and 49 United States Code 5303;
2. Title VI of the Civil Rights Act of 1964, as amended (42 United States Code 2000d-1) and 49 Code of Federal Regulations Part 21;
3. 49 United States Code 5332 prohibiting discrimination on the basis of race, color, creed, national origin, sex, or age in employment or business opportunity;
4. Section 1101(b) of the Moving Ahead for Progress in the 21st Century Act and 49 Code of Federal Regulations Part 26 regarding the involvement of disadvantaged business enterprises in United States Department of Transportation funded projects;
5. 23 Code of Federal Regulations Part 230 regarding the implementation of an equal employment opportunity program on Federal and Federal-aid highway construction contracts;
6. the provisions of the Americans with Disabilities Act of 1990 (42 United States Code 12101 et seq.) and the regulations found in 49 Code of Federal Regulations Parts 27, 37, and 38;
7. the Older Americans Act, as amended (42 United States Code 6101) prohibiting discrimination on the basis of age in programs or activities receiving Federal financial assistance;
8. Section 324 of 23 United States Code regarding the prohibition of discrimination on the basis of gender; and
9. Section 504 of the Rehabilitation Act of 1973 (29 United States Code 794) and 49 Code of Federal Regulations Part 27 regarding discrimination against individuals with disabilities.

Included in this certification package is a summary of Noteworthy Achievements by the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area and, a list of any recommendations and/ or corrective actions. The contents of this Joint Certification Package have been reviewed by the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area and accurately reflect the results of the joint certification review meeting held on May 10, 2013.

Based on a joint review and evaluation, the Florida Department of Transportation and the **Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area** recommend that the **Metropolitan Transportation Planning Process for the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area** be **Certified**.

_____ _____

Florida Department of Transportation
District Two Secretary (or designee)

Date

_____ _____

Metropolitan Transportation Planning Organization
for the Gainesville Urbanized Area Chair (or designee)

Date

North Central Florida Regional Planning Council

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Transportation Improvement Program Amendments for Fiscal Years 2012-13 to 2016-17

STAFF RECOMMENDATION

Approve the proposed Transportation Improvement Program amendments listed in Exhibit 1.

BACKGROUND

The Florida Department of Transportation has requested several amendments to the currently adopted Fiscal Years 2012-13 to 2016-17 Transportation Improvement Program (see attached Exhibit 1 email dated May 3, 2013). Right-of-way activity for the following three bicycle/pedestrian projects is being added to Fiscal Year 2013/14:

1. University of Florida Campus Greenway from State Road 121 (SW 34th Street) to Gale Lemerand Drive.

2. University of Florida Campus Greenway from Gale Lemerand Drive to Archer Road.

3. Hull Road Greenway from Hull Road Parking Lot to State Road 121 (SW 34th Street).

t:\marlie\ms13\mtpo\memo\tipamendjune3.docx

EXHIBIT 1

From: Taulbee, Karen
To: Marlie Sanderson
Cc: Mike Escalante
Subject: FW: TIP PROJECTS FOR FY 2014
Date: Friday, May 03, 2013 11:20:34 AM
Attachments: image001.png
image002.png
image003.png

Marlie,

I will need a TIP amendment agenda item for the TAC/CAC on May 22nd and the MTPO on June 3rd. The Right of way activity on all three of the Transportation Alternatives projects below are being advanced. They need to be amended into the Current TIP, which will be active until Oct 1, 2013. Thank you.
Please contact me if you have any questions.

Karen S. Taulbee, AICP
Transportation Specialist
Jacksonville Urban Office
904-360-5652
karen.taulbee@dot.state.fl.us

From: Cloud, Barbara
Sent: Friday, May 03, 2013 11:10 AM
To: Taulbee, Karen
Subject: TIP PROJECTS FOR FY 2014

In looking through the STIP amendments needed report for Alachua county, the following highway/bridge projects should be amended into the current TIP/STIP in June for FY 14

428896-1	UF CAMPUS GREENWAY FROM SR 121 (SW 34TH ST) TO GALE LEMERAND DR	RIGHT OF WAY	3311-041-P	41	01	ACTA	2014	0	1,061	1,061
				43	01	ACTA	2014	0	212	212
								0	1,273	1,273
430614-1	UF CAMPUS GREENWAY FROM GALE LEMERAND DR TO SR 24 (ARCHER RD)	RIGHT OF WAY	9886-326-A	41	01	ACTA	2014	0	1,061	1,061
				43	01	ACTA	2014	0	212	212
								0	1,273	1,273
432242-1	HULL ROAD FROM PARKING LOT TO SR 121/SW 34TH STREET	RIGHT OF WAY	9886-327-A	41	01	ACTA	2014	0	1,159	1,159
				43	01	ACTA	2014	0	234	234
								0	1,393	1,393

Barbara A. Cloud
District Two - Federal Aid Coordinator
Florida Department of Transportation
1109 South Marion Avenue
Lake City, FL 32025
(386)961-7540
barbara.cloud@dot.state.fl.us

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May 24, 2012

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Transportation Improvement Program for Fiscal Years 2013-14 - 2017-18

JOINT RECOMMENDATIONS

The Citizens Advisory Committee, Bicycle/Pedestrian Advisory Board, Technical Advisory Committee and staff all recommend approval of the Fiscal Years 2013-14 - 2017-18 Transportation Improvement Program.

BACKGROUND

Enclosed please find a draft copy of the Fiscal Years 2013-14 - 2017-18 Transportation Improvement Program. The Transportation Improvement Program is a staged implementation program of transportation projects consistent, to the maximum extent feasible, with adopted comprehensive plans of Alachua County and the City of Gainesville.

Exhibit 1 is a copy of the advertisement that appeared in The Independent Florida Alligator on Tuesday, May 14, 2013 and in the Gainesville Guardian and Gainesville Sun on Thursday, May 9, 2013. A full color copy of the draft Transportation Improvement Program may be viewed at the following website:

http://ncfrpc.org/mtpo/publications/TIP/TIPDOC13_maydft_4_web.pdf

Authorization of Funds

The Transportation Improvement Program is the most important document that is approved annually by the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area. In order for federal transportation funds to be spent in the Gainesville Metropolitan Area, they must be approved by the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area and included in this document.

Approval of the Transportation Improvement Program authorizes about $26 million in federal funds for Fiscal Year 2013/14. Of this $26 million, about $17 million are for Regional Transit System projects.

State Road 226 Transportation System Management Project- 60 Percent Plans

On March 11, 2013, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area reviewed the 60 percent plans for the State Road 226 Transportation System Management Project. At the conclusion of this discussion, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved the "60 Percent Plans" with the following two modifications (see attached Exhibit 2 letter)

1. *eliminate the sliplane in the southeast corner of the South Main Street and Southeast 16th Avenue intersection in order to increase bicycle, pedestrian and motor vehicle safety; and*

2. *provide a sidewalk that is wider than five feet on the south side of Southeast 16th Avenue in order to accommodate combined-use (bicycle and pedestrian) activity.*

Exhibit 3 is a Florida Department of Transportation letter dated April 12, 2013 stating that the sliplane will be included in the final design. In addition, this letter also discussed why a wider sidewalk cannot be constructed.

The State Road 226 Transportation System Management Project is listed in the Fiscal Years 2013/14 - 2017/18 Transportation Improvement Program on page 41. At the March 11, 2013 meeting, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area requested that staff research whether the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has the option not to include this project in its Transportation Improvement Program.

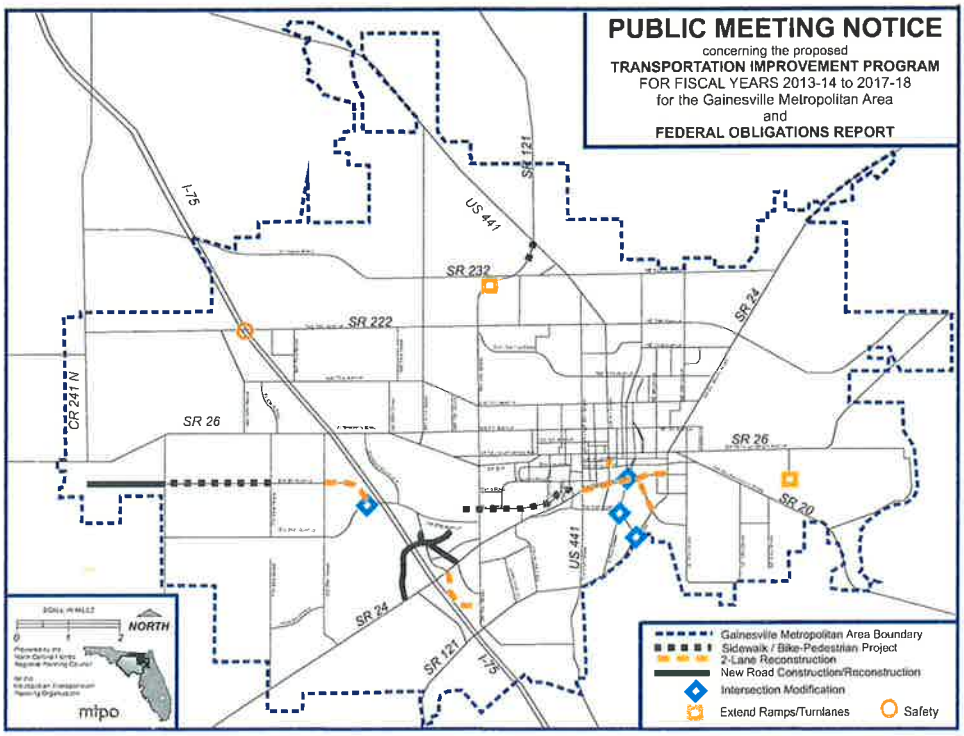
According to Chapter 339.175 (8) (d) Florida Statutes,

> *"projects included in the transportation improvement program and that have advanced to the design stage of preliminary engineering may be removed from or rescheduled in a subsequent transportation improvement program only by the joint action of the M.P.O. and the department [Florida Department of Transportation]."*

The State Road 226 Transportation System Management Project has advanced to the design stage of preliminary engineering. Therefore, both the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area and the Florida Department of Transportation will have to agree to remove this project from the Transportation Improvement Program.

t:\marlie\ms13\mtpo\memo\tipjune3.docx

EXHIBIT 1



COMMUNITY TRANSPORTATION MEETING

June 3, 2013 at 5:00 p.m.
Jack Durrance Auditorium, County Administration Building,
12 SE 1ST STREET, GAINESVILLE, FLORIDA

PURPOSE: The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has scheduled a public meeting to receive input concerning the proposed Transportation Improvement Program for Fiscal Years 2013-14 to 2017-18. The Transportation Improvement Program is a staged implementation program of transportation projects consistent, to the maximum extent feasible, with the Alachua County and City of Gainesville comprehensive plans.

Projects in the proposed Transportation Improvement Program are also consistent with the Gainesville Metropolitan Area Year 2035 Transportation Plan- The Livable Community Reinvestment Plan. This plan identifies transportation system modifications expected to be needed to serve projected volumes and patterns of traffic through the Year 2035. A final decision regarding all projects contained in the Transportation Improvement Program will be forwarded to the Florida Department of Transportation by the adoption of this Transportation Improvement Program document.

The Federal Obligations Report is included in Appendix B of the Transportation Improvement Program. This Report shows the expenditure of federal funds within the Gainesville Metropolitan Area from October 1, 2011 through September 30, 2012.

This map only shows some of the transportation projects scheduled during the next five years. The proposed Transportation Improvement Program includes transportation projects such as: bicycle; pedestrian; project development and environmental studies; resurfacing/repaving; school safety concern; transportation enhancement; and transit projects, including transportation disadvantaged projects.

THE MEETING ROOM WILL BE OPEN AT 4:30 PM FOR THE PUBLIC TO REVIEW THE PROPOSED TRANSPORTATION IMPROVEMENT PROGRAM AND STAFF WILL BE PRESENT TO ANSWER QUESTIONS.

Copies of the meeting agenda and more detailed information concerning the Federal Obligations Report and proposed Transportation Improvement Program can be obtained by writing to the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area, c/o North Central Florida Regional Planning Council, 2009 NW 67th Place, Gainesville, Florida 32653, by appearing in person at the above address during business hours, at the www.ncfrpc.org/mtpo website, or by calling 352.955.2200. All persons are advised that, if they decide to contest any decision made at this public meeting, they will need a record of the proceedings and, for such purpose, they may need to ensure that a verbatim record of the proceedings is made, which record includes the testimony and evidence upon which it is to be based. All interested persons are invited to attend and be heard. Public participation is solicited without regard to race, color, national origin, age, sex, sexual orientation, marital status, religious status, disability, familial status or gender identity. Persons who require special accommodations under the American with Disabilities Act, or persons who require translation services (free of charge), should contact Mr. Marlie Sanderson at 352.955.2200, extension 103, at least seven (7) days before the public meeting.

The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area consists of the Gainesville City Commission, the Alachua County Commission and nonvoting advisors of the University of Florida, the Florida Department of Transportation and the Alachua County League of Cities. The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area is responsible for the continuing, comprehensive and cooperative urban transportation planning program for the Gainesville Metropolitan Area. This planning program is required in order to receive federal and state funds for transportation projects.

EXHIBIT 2

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March 18, 2013

Mr. Greg Evans, P. E., District 2 Secretary
Florida Department of Transportation
1109 South Marion Avenue
Lake City, Florida 32025-5847

RE: State Road 226 (SE 16th Avenue) Transportation System Management- 60 Percent Plans

Dear Secretary Evans:

At its meeting on March 11, 2013, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area reviewed the Florida Department of Transportation "60 Percent Plans" for the State Road 226 (SE 16th Avenue) Project. At this meeting, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved the "60 Percent Plans" with two modifications:

1. *eliminate the sliplane in the southeast corner of the South Main Street and Southeast 16th Avenue intersection in order to increase bicycle, pedestrian and motor vehicle safety; and*

2. *provide a sidewalk that is wider than five feet on the south side of Southeast 16th Avenue in order to accommodate combined-use (bicycle and pedestrian) activity.*

The Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area is concerned that the proposed sliplane will create an unsafe condition for pedestrians, bicyclists and motor vehicles at this location. Therefore, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area does not support the construction of a sliplane at this location in this project.

Thank you for giving us the opportunity to review and comment on this project. If you have any questions concerning this matter, please contact Mr. Marlie Sanderson, AICP, Director of Transportation Planning at 352.955.2200, extension 103.

Sincerely,

Randy Wells, Chair
Metropolitan Transportation Planning Organization for the
Gainesville Urbanized Area

xc: James Bennett, FDOT District 2 Planning Manager
Karen Taulbee, FDOT District 2 MTPO Liaison

T:\Marlie\MS13\MTPO\LETTERS\fdotSE16Avemarch11.docx

EXHIBIT 3

Florida Department of Transportation

RICK SCOTT
GOVERNOR

1109 South Marion Avenue
Lake City, FL 32025-5874

ANANTH PRASAD, P.E.
SECRETARY

April 12, 2013

Commissioner Randy Wells, Chairman
Metropolitan Transportation Planning Organization
2009 NW 67th Place
Gainesville, FL 32653-1603

RE: SR 226 (SE 16th Avenue) intersection improvement at SR 329 (Main Street)

Dear Commissioner Wells:

Thank you for your recent letter dated March 18, 2013 regarding the upcoming project to modify the intersection of SR 226 (SE 16th Avenue) at SR 329 (Main Street). As you are aware, the Department completed a Project Development and Environmental Study (PD&E) for adding capacity on SR 226 to connect between SR 329 and SR 331 (Williston Road). The Department presented the results of the study to the MTPO in October 2010. The results indicated additional lanes were not required to meet future traffic demands provided intersection improvements were made at several locations, one of which is the intersection of SR 226 and SR 339. The Gainesville MTPO approved the results, of which included the turn lane of concern, at the October 2010 meeting.

The Department has continued to advance this project beyond the planning stage into final design and rights-of-way acquisition. The Department believes the proposed right-turn lane and slip lanes is a safe design providing benefits to motorist, pedestrians and bicyclist. As such, we believe it is appropriate to move forward with the current design. The Department has also reviewed the request for wider sidewalks within the limits of the project. Issues related to the existing utilities and drainage prevent widening the sidewalk to the desired width expressed in your letter.

The Department appreciates your interest and comments by the Gainesville MTPO. If you have any further questions or need any additional information, please contact James Bennett by phone at (904) 360-5646 or via email at James.Bennett@dot.state.fl.us.

Respectfully,

Greg Evans

Greg Evans, P.E.
District Two Secretary

CC: James G. Bennett, P.E., Urban Transportation Development Manager
Karen Taulbee, AICP, Gainesville MTPO Liaison

North Central Florida Regional Planning Council

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: List of Priority Projects

JOINT RECOMMENDATIONS

The Citizens Advisory Committee, Technical Advisory Committee and staff all recommend approval of the Fiscal Years 2015 to 2019 List of Priority Projects.

BICYCLE/PEDESTRIAN ADVISORY BOARD RECOMMENDATION

The Bicycle/Pedestrian Advisory Board recommends approval of the Fiscal Years 2015 to 2019 List of Priority Projects with one exception being to list updating the Bicycle Facilities Map of North Central Florida series brochures as Priority No. 1 on Table 1A (see Exhibit 1).

BACKGROUND

Each year, the MTPO develops recommended transportation priorities for projects that are needed, but not currently funded (or fully-funded). This information is used by the Florida Department of Transportation each fall to develop its Tentative Five Year Work Program.

A full color copy of the draft *List of Priority Projects* can be viewed at the following website link:

http://ncfrpc.org/mtpo/publications/TIP/LOPP13dft_4_web.pdf

Exhibit 2 shows the Bicycle Facilities Map of North Central Florida series brochures.

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Exhibit 1

Table 1A
Bicycle/Pedestrian Priorities - Transportation Alternatives Project Fund
Fiscal Years 2014-15 to 2018-19
(within the Gainesville Metropolitan Area)

Number	Project	Location	Description
1	Bicycle Facilities Map for North Central Florida	AT: NCFRPC Regionwide	Update bicycle map series brochures
2	SW 27 Street/SW 40 Place/ SW 25 Terrace	FM: SW 35 Terrace TO: Williston Road	Construct ADA-compliant multi-use path
3	E University Avenue [SR 26]	FM: E 9 Street TO: Waldo Road [SR 24]	Pedestrian refuge islands
4	Norton Elementary Trail	FM: NW 39 Avenue TO NW 45 Avenue	Construct bicycle/pedestrian trail
5	NE 15 Street	FM: NE 12 Avenue TO: NE 16 Avenue	Construct ADA-compliant sidewalk
6	NW 2 Street	FM: NW 10 Avenue TO: NW 14 Avenue	Construct ADA-compliant sidewalk
7	Bus Stop Upgrades	AT: RTS Systemwide	Construct bus stops and sidewalk connections
8	SW 40 Boulevard/ SW 47 Avenue	FM: Archer Road TO SW 34 Street	Construct bicycle/pedestrian trail
9	E 10 Street	FM: Depot Avenue Trail TO: NE 3 Avenue	Construct bicycle/pedestrian trail; add refuge island at NE 3 Avenue/ Waldo Road intersection
10 *Partially Funded*	*SW 43 Street*	*FM: SW 40 Boulevard* *TO: SW 20 Avenue*	*Construct ADA-compliant sidewalk*
11	NW 3 Street	FM: W University Avenue TO: NW 8 Avenue	Construct ADA-compliant sidewalk
12	SW 34 Street Grade-Separated Crossing*	AT: SW 34 Street [SR 121]	Construct bicycle/pedestrian grade-separated crossing
13	Hull Road Connector*	FM: SW 20 Avenue TO: End of Parking Area	Construct bicycle/pedestrian trail
14	Lake Kanapaha Trail	FM: Tower Road TO: Interstate 75	Construct bicycle/pedestrian trail

Note: Projects in italic text are partially funded, as shown in the Transportation Improvement Program.
*2004 Alachua Countywide Bicycle Master Plan Addendum- Archer Braid projects

ADA = Americans with Disabilities Act of 1990; E = East; FM = From; NCFRPC = North Central Florida Regional Planning Council; NW = Northwest; RTS = Regional Transit System; SW = Southwest; UF = University of Florida; W = West

Initial Enhancement Priorities were developed by the Bicycle/Pedestrian Advisory Board.

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EXHIBIT 2

On the northern part of the region, below the State border is **Jasper**, is the home of the Hamilton County Historical Museum. Nearby is **White Springs**, a charming community with many historic buildings such as the Telford House located on the Suwannee River where the wealthy and famous vacationed during the turn of the century. This quaint small town is a major draw for canoeists and bicyclists.

While in **White Springs** visit the Stephen Foster State Folk Culture Center, a wooded park on the Suwannee River which honors the composer who immortalized the river through his song "Old Folks At Home". Florida folk crafts are displayed and demonstrated year round on the park's craftsmen' sojourneywalk.

Lake City is the location of the Florida Sports Hall of Fame and Florida Tourist Welcome Center. While in **Lake City** visit the Columbia County Historical Museum located in an 1870s home furnished with authentic period antiques and Civil War artifacts.

A short drive from east from **Lake City** is the Olustee Battlefield State Historic Site, scene of the Battle of Olustee in 1864 and the location of the battles annual reenactment. The Battlefield is located within the Osceola National Forest, with pine flatwoods, cypress swamps, a lake and miles of Nature trails where native Florida wildlife roam free.

Travel to Live Oak along the historic DeSoto Trail, the route taken in 1539 by Hernando DeSoto and his Spanish troops as they searched in vain for gold and treasure. In **Live Oak** enjoy the regional architecture and visit the Suwannee County Historical Museum located in a former train station.

North of **Live Oak** is the Spirit of the Suwannee Park where country-western, bluegrass, and gospel music festivals are held year round.

Roads leading to **Madison** take you through peach orchards, scenic tree farms and tobacco fields. The classically beautiful homes in Madison, such as the Wardlaw-Smith Mansion on Base Street Are reminders that this was once a cotton plantation society. Nearby, Cherry Lake and Blue Springs on the Withlacoochee River are Great places for a family picnic and a dip in the cool waters.

For a sense of life gone by, visit the authentic Cracker farm at the Forest Capital Museum in **Perry**, a city known for its timberlands, Turpentine, and sawmills.

To the west one will find and enjoy the Gulf Coast. These shorelines, hidden coves, and bayous offer a fishing village ambience and sheltered waters for scalloping, boating, and saltwater fishing. Try **Keaton Beach** and **Steinhatchee** to experience some of the best sunset vistas and seafood restaurants around. These friendly fishing communities offer visitor services such as marinas, charter and guided fishing, and victorian-style condo rentals.

Travel to **Old Town** site of the "City of Hawkinsville" Underwater Archaeological Preserve. On the bottom of the Suwannee River and accessible to divers, are the remains of a paddle wheel steamship which sunk in the early 1900's.

The **Town of Suwannee**, at the mouth of the Suwannee River is a great place to rent a houseboat, a canoe, or a fishing boat and explore the river delta or the Gulf shoreline, which is still much as it was during the days of Florida's early settlers. From **Suwannee**, boaters may take a leisurely ride upstream for water access to nearby springs.

Discover Manatee Springs State Park , a family recreation area, where gentle manatees are often seen playing in the cool, clear water. Travel through **Fanning Springs**, on CR 232 to Hart Springs, a family-oriented campground, or Otter Springs RV resort, both located on the Suwannee River.

Trenton is the home of West Central Florida Railroad and the site of an authentically restored train depot.

Branford, a diving mecca, is well known to divers from many countries who come to dive the springs and explore the caverns of a vast underwater world. Diving instruction and guided tours are available from master dive instructors in **Branford** and nearby dive shops.

West of **Branford** is **Mayo** whose percentage of Irish descendants is one of the largest in the country. A short distance from **Mayo**, off Convict Springs Road, is Jim Hollis' River Rendezvous, a popular camping resort located on the Suwannee River.

Cool off at the Ichetucknee Springs State Park. A delightful spot to learn the easy art of "tubing" on a crystal clear river that carries you gently through a pristine hardwood forest.

Leave plenty of time to sample the other outstanding springs in the area. Choose a campsite such as Ginnie Springs, a camping resort with dive shop and equipment rentals. Or retire in the delightful ambience of one of the many bed and breakfast inns nearby. This area has become very popular with canoeists, bicyclists and nature lovers.

While in **Levy County** visit the nostalgic Suwannee, Waccasassa, and Withlacoochee Rivers, cool off at **Fanning Springs** or **Manatee Springs**, and "play-ball" at the Lake Rousseau and Cross Florida Greenbelt State Recreation Areas. If you're looking for solitude or are interested in bird watching try the Andrews, Gulf Hammock, **Fanning Springs**, and **Cedar Key** National Wildlife Preserves. Interesting ecological/geological formations can be studied at the Levy County Forest/Sandhills, Big Bend Seagrass Aquatic Preserve, and the **Cedar Key** Scrub State Park. For those more interested in historical and archaeological destinations try **Cedar Key** Historical and Archaeological District, the Sea Horse Key & Lighthouse, the Salt Works at Salt Island, the Shell Mounds in the Lower Suwannee Refuge, and Fort's Waccasassa and Jennings. Or if you're just interested in the perfect trail try the Nature Coast Greenway and Goethe State Forest. Remember to take time to relax and enjoy the many resources available in Levy County as part of Florida's Nature Coast.

East of the Santa Fe River is historic **High Springs**, an outstanding community of antique shops, arts and crafts studios, bed and breakfast inns, quaint cafes, and historic buildings. High Springs has blossomed as a tourist attraction because of its historic renovations and old-time charm.

North of **High Springs** is O'Leno State Park where the Santa Fe River disappears into a sinkhole and flows underground for nearly three miles before resurfacing.

A pleasant country drive will take you to **Lake Butler**, where you can admire the many historic homes or fish in the lake which gave the town its name.

In nearby **Starke**, visit the collection of curio shops along historic Call Street. Or browse through the Gene Matthews Historical Museum where you can learn about **Starke's** colorful role as a playground to WWII soldiers stationed at Camp Blanding.

Stop at the old railroad town of **Waldo** to browse in its red brick antique shops and large flea market, then on to the friend **Town of Hawthorne**. While in this area try fishing in one of the many lakes such as Orange and Lochloosa, renowned for their abundance of bass. In **Hawthorne** pick up the eastern end of the 17mile **Gainesville/Hawthorne** State Trail, a popular pathway for hikers, bicyclists, runners, inline skaters, and horseback riders. The trail, a converted railroad route, borders the edge of Paynes Prairie, a state owned wildlife sanctuary with wild stallions, bison, deer, alligators, and a variety of migratory birds.

Gainesville, home of the University of Florida, is the cultural capital of north central Florida. The city also offers an outstanding range of collegiate sports and athletic events. The Florida Museum of Natural History is located on the campus, as are the Harn Museum of Art and the Center for the Performing Arts. Downtown Gainesville is a lively entertainment complex with the Hippodrome Theatre, sidewalk cafes, night clubs, artists' studios, fine restaurants, cozy pubs and open air festivals.

A Few mile south of **Gainesville** is historic **Micanopy**, a charming community with a againing popularity with Hollywood film makers. A stroll down its shady streets will lead you to an array of hand crafters and antique shops full of treasures of yesteryear. Southeast of **Micanopy** is the **Cross Creek** home of Marjorie Kinnan Rawlings, author of The Yearling, and many other famous novels. Guided tours of this historic Cracker home are given year round.

Travel north of **Gainesville** to the town of **Alachua**, to enjoy the shops and restaurants on historic Main Street. Towns of **Newberry** and **Archer**, each offer their own style and country charm.

Published by the

NORTH CENTRAL FLORIDA REGIONAL PLANNING COUNCIL
2009 NW 67th Place, Suite A
Gainesville, FL 32653-1603
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Email: mpton@ncfrpc.org

With Funding Assistance from the FLORIDA DEPARTMENT OF TRANSPORTATION DISTRICT TWO

October 1998

Charles F. Justice
Executive Director

Marlie Sanderson
Director of Transportation Planning

Gerry Dedenbach
Project Coordinator / Senior Transportation Planner

Kevin Parrish
Database Coordinator

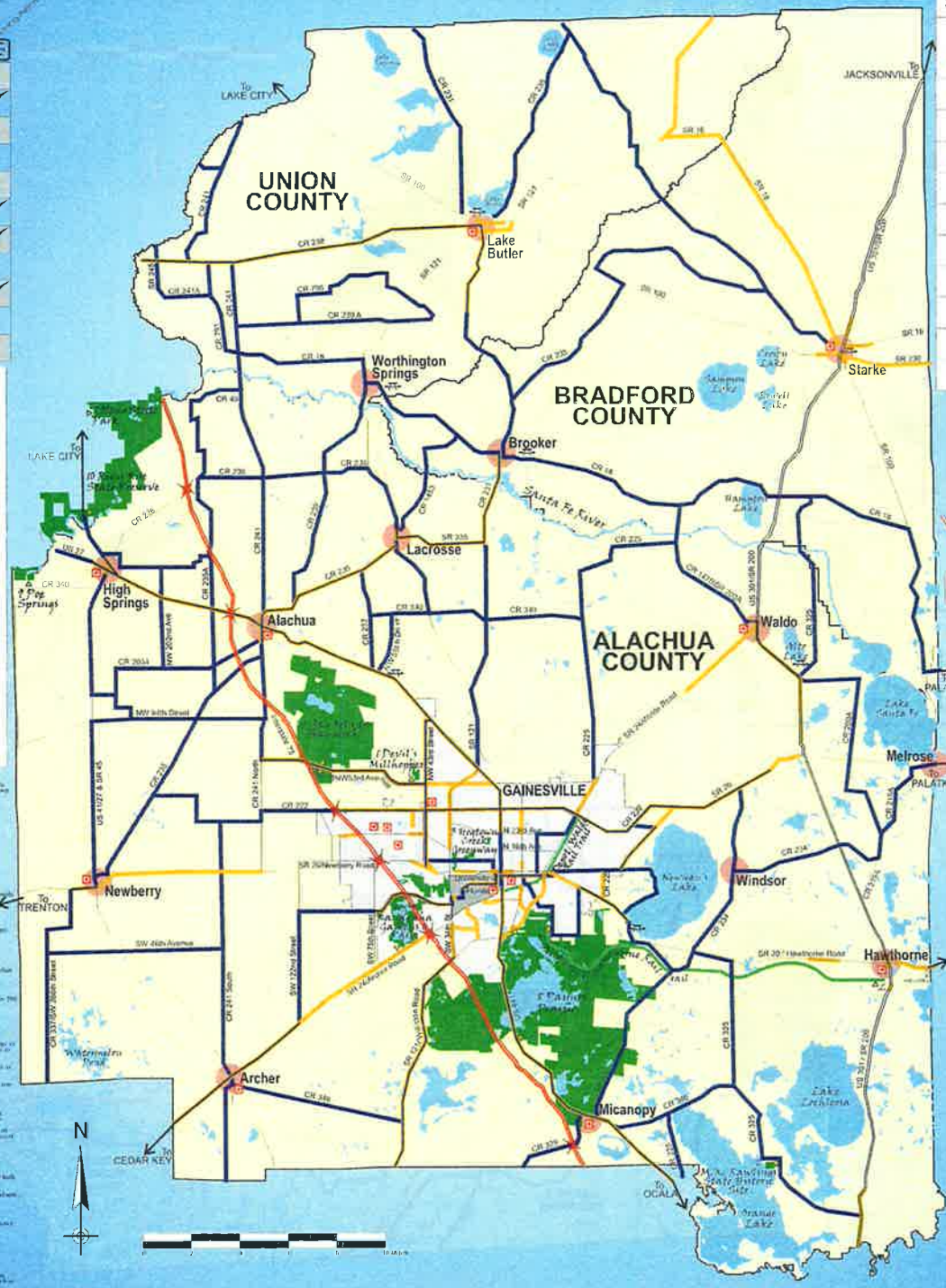
LEGEND

SYMBOL	MEANING	SYMBOL	MEANING
	Roadway		County Boundary
	Bicycle Facility Present (Greater than 3 feet)		Park or Recreational Facility
	Cycling Club Preferred Route		Water Body
	Bicycle Facility Present on Preferred Route		Emergency Care Location
	Interstate Highway		Entrance
	Major Divided Highway		Picnic Area

BICYCLE FACILITIES MAP FOR NORTH CENTRAL FLORIDA

RECREATIONAL FACILITIES INDEX						
1. DEVILS MILLHOPPER STATE GEOLOGICAL SITE	✓	✓				
2. GAINESVILLE HAWTHORNE STATE RAIL TRAIL	✓	✓	✓		✓	✓
3. HOGTOWN CREEK GREENWAY	✓	✓				
4. KANAPAHA PARK		✓				
5. KANAPAHA BOTANICAL GARDENS	✓	✓				
6. M.K. RAWLINGS STATE HISTORIC SITE		✓				✓
7. OLENO STATE PARK	✓	✓	✓	✓	✓	✓
8. PAYNES PRAIRIE STATE PARK	✓	✓				
9. POE SPRINGS	✓	✓		✓		✓
10. RIVER RISE STATE PARK		✓				
11. SAN FELASCO HAMMOCK	✓	✓			✓	

Alachua, Bradford and Union Counties Topographic Relief Map



SYMBOL	MEANING
	Roadway
	Bicycle Facility Present (Greater than 3 feet)
	Cycling Club Preferred Route
	Bicycle Facility Present on Preferred Route
	Interstate Highway
	Major Divided Highway
	County Boundary
	Park or Recreational Facility
	Water Body
	Emergency Care Location
	Entrance
	Picnic Area

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For sources of free and discounted helmets throughout Florida call:
Sandra G Decker
Director of Prevention Programs
Brain Injury Association of Florida, Inc
(561) 965-2110

BICYCLE FACILITIES MAP FOR NORTH CENTRAL FLORIDA

RECREATIONAL FACILITIES INDEX

1. BIG SHOALS STATE FOREST	✓
2. FALMOUTH SPRING	✓
3. OAK POND	✓
4. [illegible]	✓
5. HOLTON CREEK WMA	✓
6. FORT PARKNEE SPRINGS	✓
7. LITTLE RIVER SPRINGS	✓
8. LITTLE SHOALS	✓
9. OLENO STATE PARK	✓
10. OSCEOLA NAT. FOREST	✓
11. PEACOCK SLOUGH	✓
12. PEACOCK SPRINGS STATE REC. AREA	✓
13. POT SPRINGS	✓
14. [illegible] STATE PRESERVE	✓
15. ROYAL SPRINGS	✓
16. SPIRIT OF THE SUWANNEE	✓
17. STEPHEN FOSTER STATE FOLK CULTURE CENTER	✓
18. SUWANNEE RIVER STATE PARK	✓
19. SUWANNEE SPRINGS	✓
20. SUWANNEE TRAIL AT BRANFORD	✓

LEGEND

SYMBOL	MEANING	SYMBOL	MEANING
	Roadway		County Boundary
	Bicycle Facility Present (Greater than 3 feet)		Park or Recreational Facility
	Cycling Club Preferred Route		Water Body
	Bicycle Facility Present on Preferred Route		Emergency Care Location
	Interstate Highway		Entrance
	Major Divided Highway		Picnic Area

As of January 1, 1997, children in Florida under age 16 must wear a bicycle helmet that is properly fitted, fastened and meets safety standards.

Before wearing a helmet, look inside for the sticker that says it meets the safety standards of ANSI Z90.4 or Snell Memorial Foundation (1984) or Consumer Product Safety Commission (1997).

In Florida a bicyclist must follow the same rules as other vehicle operators:
This means you must:
1. Ride with traffic on the right side of the road and use hand signals
2. Stop for stop signs and red lights
3. Don't ride with a headset on, you won't be a singer coming
4. Use lights and reflectors at night

For information on the rules of Florida's bicycle laws, write or call:
State Bicycle Program
Department of Transportation
605 Suwannee Street, MS 82
Tallahassee, FL 32399-0450
(850) 487-1200

For statistics of injury and educational helmets throughout Florida call:
Sandra G. Bichan
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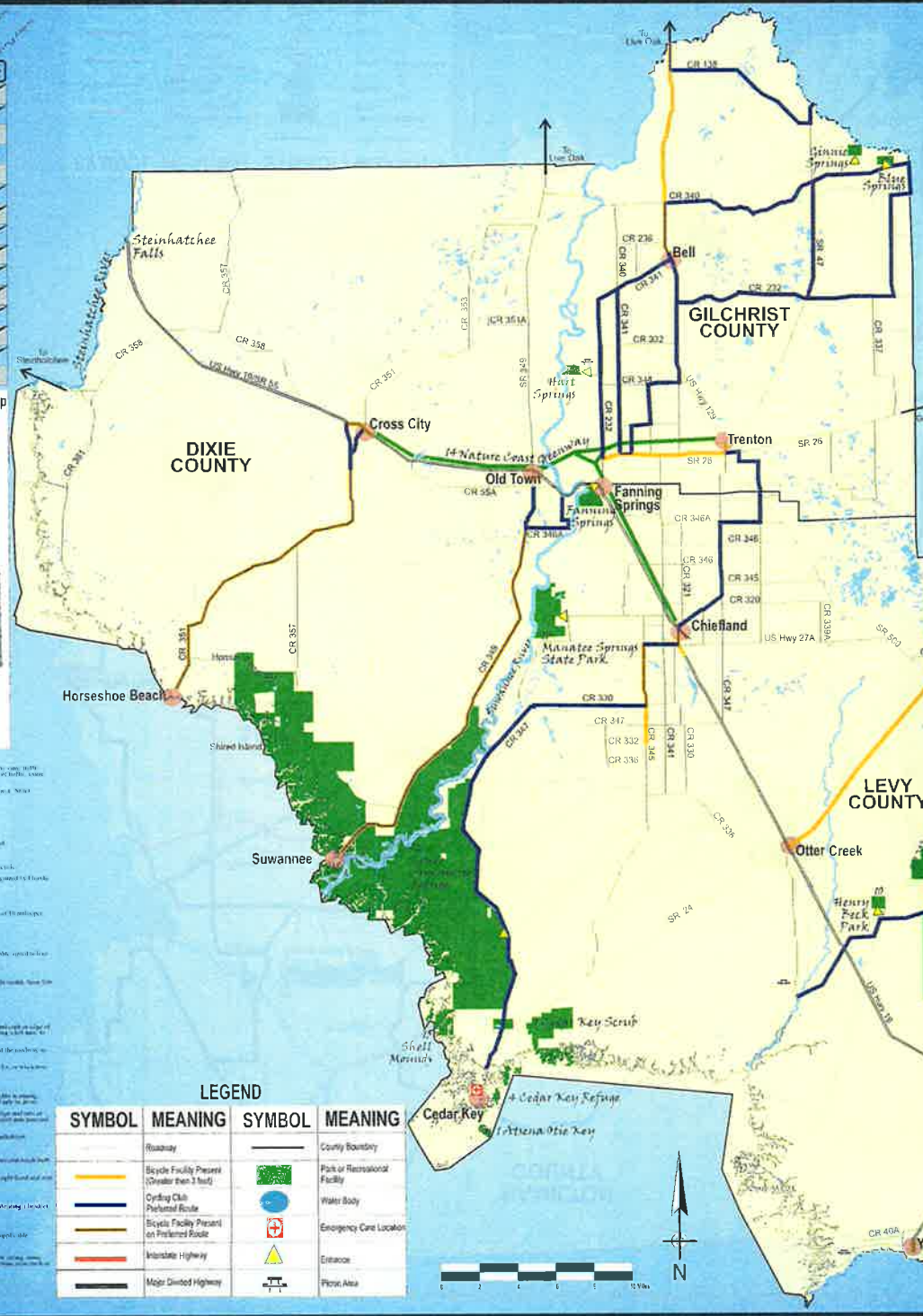
Gerry Dedenbach
Project Coordinator /
Senior Transportation Planner

Kevin Parrish
Database Coordinator

RECREATIONAL FACILITIES INDEX

1. ATSENA OTIE KEY	✓	✓		✓		✓
2. BLUE SPRINGS (GILCHRIST COUNTY)		✓		✓	✓	✓
3. BLUE SPRINGS (LEVY COUNTY)		✓		✓		
4. CEDAR KEY REFUGE		✓				
5. CEDAR KEY SCRUB		✓				
6. FANNING SPRINGS	✓	✓		✓		✓
7. GINNIE SPRINGS		✓		✓	✓	✓
8. GOETHE STATE FOREST	✓	✓		✓	✓	✓
9. HART SPRINGS		✓		✓		✓
10. HENRY BECK PARK		✓		✓		✓
11. LAKE ROUSSEAU STATE REC AREA		✓		✓		✓
12. LOWER SUWANNEE REFUGE	✓	✓	✓	✓	✓	✓
13. MANATEE SPRINGS STATE PARK	✓	✓	✓	✓	✓	✓
14. NATURE COAST GREENWAY	✓	✓	✓	✓		✓
15. SHELL MOUNDS		✓				

Dixie, Gilchrist and Levy Counties Topographic Relief Map



LEGEND

SYMBOL	MEANING	SYMBOL	MEANING
	Roadway		County Boundary
	Bicycle Facility Present (Greater than 3 feet)		Park or Recreational Facility
	Cycling Club Preferred Route		Water Body
	Bicycle Facility Present on Preferred Route		Emergency Care Location
	Interstate Highway		Entrance
	Major Divided Highway		Picnic Area

0 2 4 6 8 10 Miles

As of January 1, 1997, children in Florida under age 16 must wear a bicycle helmet that is properly fitted, is fastened with a strap and meets safety standards

Helmet use may reduce head injury for the whole bicycle rider if meets the safety standards of: ANSI Z-90, Snell Memorial Foundation (1984), or Consumer Product Safety Commission (1987).

If Parents who collect must follow the same safety bicycle operations.
This means you must:
1. Ride on the right side of the road and use hand signals
2. Stop for stop signs and red lights
3. Have a side with a bicycle (or motorcycle) have damage correctly
4. Use lights and reflectors at night

For information on the use of
Florida's Bicycle Laws, write or call
State Bicycle Program
Department of Transportation
605 Suwannee Street, MS 62
Tallahassee, FL 32399-0450
(850) 487-1200

For sources of free and discounted helmets throughout Florida call:
Sandra G. Becker
Director of Prevention Programs
Brain Injury Association of Florida, Inc
(561) 983-2146

BICYCLE FACILITIES MAP FOR NORTH CENTRAL FLORIDA

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DISTRICT TWO

October 1998

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Executive Director

Marlie Sanderson
Director of Transportation Planning

Gerry Dedenbach
Project Coordinator /
Senior Transportation Planner

Kevin Parrish
Database Coordinator

RECREATIONAL FACILITIES INDEX

1. ANDERSON SPRINGS WMA	✓				
2. BIG BEND WMA	✓		✓		
3. ECONFINA BRIDGE	✓				
4. HAGENS COVE	✓		✓		✓
5. HICKORY MOUND UNIT WMA	✓				
6. JIM HOLLIS' RIVER RENDEZVOUS	✓		✓		✓
7. KEATON BEACH	✓		✓		✓
8. LAFAYETTE BLUE SPRINGS	✓				
9. MADISON BLUE SPRINGS	✓		✓		
10. ST MARKS NATIONAL WILDLIFE REFUGE	✓				
11. STEINHATCHEE FALLS	✓				✓
12. STEINHATCHEE RIVER	✓	✓			✓
13. SUWANNEE BLUFF COUNTY PARK	✓		✓		✓

Taylor, Lafayette and Madison Counties Topographic Relief Map

LEGEND

SYMBOL	MEANING	SYMBOL	MEANING
	Roadway		County Boundary
	Bicycle Facility Present (Greater than 3 feet)		Park or Recreational Facility
	Cycling Club Preferred Route		Water Body
	Bicycle Facility Present on Preferred Route		Emergency Care Location
	Interstate Highway		Entrance
	Major Divided Highway		Picnic Area

As of January 1, 1997, children in Florida under age 16 must wear a bicycle helmet that is properly fitted, is fastened with a strap, and meets safety standards.

Before you buy a helmet look inside for the sticker that says it meets the safety standards of ANSI (Z90.4) or Snell Memorial Foundation (1994) or Consumer Product Safety Commission (1997).

In Florida a bicyclist must follow the same rules as other vehicle operators.
This means you must:
1. Ride with traffic on the right side of the road and use hand signals.
2. Stop for stop signs and red lights.
3. Don't ride with a headset on, you won't hear danger coming.
4. Use lights and reflectors at night.

For information on the rest of Florida's bicycle laws, write or call:
State Bicycle Program
Department of Transportation
605 Suwannee Street, MS82
Tallahassee, FL 32399-0450
(850) 487-1200

For sources of free and inexpensive helmets throughout Florida call:
Sandra G Hunter
Director of Prevention Programs
Brain Injury Association of Florida, Inc
(561) 392-2116

North Central Florida Regional Planning Council

VII
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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Regional Transit System Alternatives Analysis Study

STAFF RECOMMENDATION

No action required. This material is for information only.

BACKGROUND

Exhibit 1 is a letter from Mr. Jesus Gomez, City of Gainesville Regional Transit System Director, requesting the opportunity to provide updated information concerning the Regional Transit System Alternative Analysis Study.

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EXHIBIT 1

CITY OF GAINESVILLE
every path starts with passion
FLORIDA

Regional Transit System
PO Box 490, Station 5
Gainesville, FL 32602-0490
(352) 334-3676
(352) 334-3681 (fax)
www.go-rts.com

May 7, 2013

Randy Wells
MTPO Chair
North Central Florida Regional Planning Council
2009 NW 67th Place
Gainesville, FL 32653-1603

Dear Chairman Wells:

As you know, the Regional Transit System (RTS) is undertaking an Alternatives Analysis Study, headed by Parsons Brinckerhoff, to identify and evaluate premium transit service improvements within the greater Gainesville area. A vital part of our public and agency involvement in the study is to brief local officials on progress made, and in the end, to secure approval of a Locally Preferred Alternative (LPA) from the Metropolitan Transportation Planning Organization (MTPO).

At the outset of the study last December and January, we conducted one-on-one briefings with City of Gainesville City Commissioners and Alachua County Board of County Commissioners. We would like to provide updated information to commissioners in a joint setting at the June 3rd MTPO meeting. The focus of this presentation will be to review the results of the screening evaluation of alternatives to date, to review public input on the study, and confirm the refined alternatives for detailed evaluation. A follow up meeting will then be held later this year to provide a more robust presentation of the draft LPA to the MTPO Advisory Committees, and to present an Action Item to the Board to approve the premium transit investment that appears to have consensus, for incorporation into the MTPO Long Range Transportation Plan.

We are prepared to share a summary of the study status and the final PowerPoint with MTPO staff and the MTPO at your earliest convenience prior to the June 3rd meeting.

Sincerely,

Jesus Gomez
RTS Transit Director

cc: Matthew Muller
Alan Danaher
Ginger Corless

North Central Florida Regional Planning Council

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Marking Pedestrian Crossings

STAFF RECOMMENDATION

No action required. This material is for information only.

BACKGROUND

At the December 3, 2012 meeting, the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area approved a motion to *"discuss at a future meeting marking unmarked pedestrian crossings on roads within high pedestrian activity areas, such as along University Avenue near the University of Florida."* Background information for this agenda item includes a Florida Department of Transportation Design Webinar presentation entitled *"Improving Pedestrian Crossings.* This presentation is located at the following link-

http://ncfrpc.org/mtpo/FullPackets/MTPO/2013/FDOT_Crosswalk_Design_Jun3.pdf

Some examples of pedestrian crossings that should be considered for "ladder striping include the following locations on West University Avenue:

NW 14th Street
NW 15th Terrace
NW 16th Street
NW 19th Street
NW 25th Street

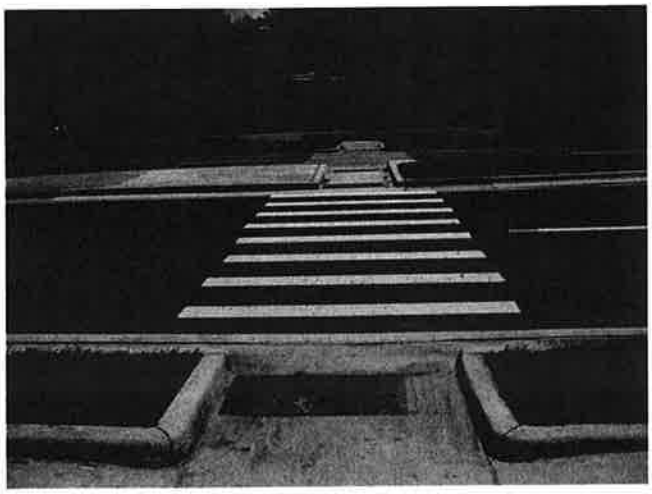

Figure 1- Ladder Striping on Waldo Road

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North Central Florida Regional Planning Council

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May 27, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Four-Laning of Archer Road- Tower Road to the City of Archer

STAFF RECOMMENDATION

No action required. This material is for information only.

BACKGROUND

A member of the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has requested the following agenda item for the June 3, 2013 meeting- *"What would it take and cost to four-lane Archer Road to the City of Archer?"*

Archer Road (State Road 24) currently reduces from four lanes to two lanes immediately west of Tower Road. From this point, Archer Road remains a two-lane road for about seven miles to the City of Archer. About 1.5 miles of this road segment is located within the metropolitan planning area boundary.

The latest Florida Department of Transportation construction cost estimates (not including right-of-way) to add two lanes to a two-lane road is about $7.4 million per mile. Therefore, the total construction cost estimate for this project is approximately $52 million. The construction cost estimate for the 1.5 mile portion in the metropolitan planning area boundary is approximately $11 million. Sufficient right-of-way for the four-laning of Archer Road was purchased about 20 years ago. However, additional right-of-way may be needed for this project to meet current design and drainage standards.

The four-laning of Archer Road is currently identified as a needed project in the adopted Year 2035 Roadway Needs Plan (see attached Table 61). However, this project is not included in the adopted Year 2035 Roadway Cost Feasible Plan (see attached Table 65). In order for this project to be eligible for construction funds, the 1.5 mile portion within the metropolitan planning area boundary must be included in Table 65. This could occur in one of the following two ways-

Option 1- Table 65 could be amended to include this $11 million project. However, other projects worth $11 million would need to be deleted from Table 65 in order for this Plan to remain financially feasible (future project costs must equal projected revenues).

Option 2- The long range transportation plan is scheduled to be updated by October 26, 2015. This project could be addressed during the plan update over the next two years.

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Table 61: Year 2035 Roadway Needs Plan Project Costs

Facility/Location	From/To	Type	Length	Project Cost 2010 $'s (in millions)
Airport Access Road	Waldo Rd to Airport	New 2 lane road	0.5	$ 2.4
Archer Road	West of I-75 to Archer (city limits)	Add 2 lanes (2 to 4)	7.0	$ 45.4
Hull Road Extension	SW 34th St to SW 43rd St Ext	New 2 lane road	1.1	$ 4.8
Radio Road Extension	SW 34th St. to Hull Rd Extension	New 2 lane road	1.0	$ 4.5
Springhills Boulevard	NW 83rd St Ext to NW 115th St	New 2 lane road	2.3	$ 20.6
Tower Road	SW 8th Avenue to Archer Road	Reconstruct (2 lane upgrade)	3.2	$ 13.4
University Avenue	NW 34th St to Waldo Rd	Multimodal Emphasis	3.7	$ 20.0
Waldo Road Multiway Boulevard	University Avenue to NE 39th Street	New 2 lane road	2.6	$ 15.9
Williston Road	West of I-75 to SW 62nd Ave	Add 2 lanes (2 to 4)	0.8	$ 5.0
NE 39th Avenue (SR 222)	Gainesville Regional Airport to NE 27th Ave	Add 2 lanes (2 to 4)	1.7	$ 10.8
NW 122nd Street Extension	NW 46th Ave to Newbery Rd	New 2 lane road	2.2	$ 9.8
NW 23rd Avenue	NW 55th St to NW 98th St	Add 2 lanes (2 to 4)	2.7	$ 17.6
NW 23rd Avenue Extension	NW 98th St to NW 143rd St (CR 241)	New 2 lane road	3.1	$ 24.1
NW 34th Street	NW 16th Ave to US 441	Add turn lanes	3.7	$ 6.0
NW 34th Street/SR121	NW 58th Ave to NW 67th Place	Add 2 lanes (2 to 4)	0.7	$ 4.5
NW 76th Boulevard Extension	NW 76th Blvd to Ft Clarke Blvd.	New 2 lane road	0.6	$ 2.8
NW 83rd Street Extension	NW 39th St to Millhopper Rd	New 2 lane road	1.5	$ 6.7
NW/SW 13th Street	SW 16th Ave to NW 23rd Ave	Multimodal Emphasis	2.6	$ 10.0
SE 4th Avenue	Depot Ave to Williston Rd	Reconstruct (2	0.7	$ 0.8

Facility/Location	From/To	Type	Length	Project Cost 2010 $'s (in millions)
		lane upgrade)		
SE 16th Avenue	Main St to Williston Rd	Add 2 lanes (2 to 4)	0.6	$ 12.5
SW 8th Avenue Extension	SW 122nd St to SW 143rd (CR 241)	New 2 lane road	1.4	$ 6.4
SW 20th Avenue	SW 34th Ave to SW 43rd St	Add turn lanes	1.0	$ 1.5
SW 20th Avenue	SW 43rd St to SW 62nd Blvd	Add 2 lanes (2 to 4)	0.6	$ 4.0
SW 23rd Terrace Extension to University of Florida campus	Hull Rd to Archer Rd	New 2 lane road	0.3	$ 1.1
SW 45th Street	Archer Rd to I-75	New 2 lane road	0.6	$ 2.7
SW 47th Street Extension	SW 47th St to SW 40th Place	New 2 lane road	0.45	$ 2.0
SW 57th Road	SW 75th Street to SW 63rd Boulevard	New 2 lane road	2.24	$ 10.1
SW 62nd Boulevard*	Newberry Rd to SW 20th Ave	Add 2 lanes (2 to 4)	1.7	$ 15.9
SW 62nd Boulevard Extension*	SW 20th Ave to Windmeadows Blvd	New 4 lane road	1.0	$ 75.1
SW 63rd/SW 67th Avenue	SW 24th Ave to Archer Road	New 2 lane road	1.9	$ 15.9
I-75 @ SR 222/39th Avenue		SIS Interchange Modification		$ 1.6
I-75 @ SR 24/Archer Road		SIS Interchange Modification		$ 1.6
I-75 @ SR 26/Newberry Road		SIS Interchange Modification		$ 1.6
I-75 @ SR 331/Williston Road		SIS Interchange Modification		$ 1.6
Total Roadway Needs				$ 372.3

Table 65: Year 2035 Roadway Cost Feasible Plan

Priority	Description	From/To	Length (In Miles)	Estimated Cost In Millions (In 2010 Dollars)
STRATEGIC INTERMODAL SYSTEM (SIS) (Cost Feasible Plan Revenues = $6.4 Million)				
-	Interstate 75 Interchange Modifications	At Williston Road At Archer Road At Newberry Road At NW 39th Ave	-	$6.4
TOTAL STRATEGIC INTERMODAL SYSTEM				$6.4
STATE HIGHWAY SYSTEM (Cost Feasible Plan Revenues = $92.0 million year of expenditure dollars)				
1	State Road 226 (SE 16th Avenue) widen to four lanes	Main Street to Williston Road	0.6	$15.0
2	State Road 121 (NW 34th Street)-construction of turn lanes to improve safety and traffic flow	NW 16th Avenue to US 441	3.5	$6.0
3	State Road 26 (University Avenue) Multimodal Emphasis Corridor[a]	Gale Lemerand Drive to Waldo Road	1.5	$4.75
4	US 441 (W. 13th Street) Multimodal Emphasis Corridor Study [a]	NW 33rd Avenue to Archer Road	2.8	$4.75
5	Waldo Road Multiway Boulevard redesign to support bus rapid transit , multi-trail and corridor redevelopment study (PD&E) [b]	University Avenue to NE 39th Avenue	2.5	$3.0
6	Bus Rapid Transit (BRT) Corridor Infrastructure-Partial	Santa Fe Village to Gainesville Regional Airport	14.0	$28.0

Priority	Description	From/To	Length (In Miles)	Estimated Cost In Millions (In 2010 Dollars)
7	State Road 24 (Archer Road) BRT Dedicated Lane(s) design, additional roadway capacity and corridor management study (PD&E)	MTPO Boundary to SW 45th Street	3.5	$0.5
8	State Road 221 (Williston Road) additional roadway capacity and corridor management (PD&E)	SW 62nd Avenue to SW 35th Way	0.5	$0.5
TOTAL STATE HIGHWAY SYSTEM				**$62.5**
Alachua County Transit and Roadway Projects (local funds identified as Cost Feasible by the Year 2020)				
1	SW 20th Avenue, four laning and multi-use path	SW 52nd Blvd to SW 61st Blvd	0.5	$8.8
2	SW 8th Avenue-Phase 2, two lane roadway and multi-use path	SW 122nd Street to SW 143rd Street	0.7	$2.7
3	NW 23rd Avenue, four laning and resurfacing	NW 51st Street to NW 59th Terrace	0.7	$1.8
4	NW 23rd Avenue, four laning	NW 83rd Street to Ft. Clarke Blvd.	0.5	$12.0
5	SE 43rd Street, construction of two-way left turn lanes, multi-use path and signalization	SR 26 (University Avenue) to SR 20 (Hawthorne Road)	1.1	$0.9
6	SW 45th / 47th Street, new roadway with travel lanes, BRT Dedicated Transit Lanes and multi-use path	Archer Road to SW 30th Avenue	0.8	$4.5
7	SW 30th Avenue, new Interstate 75 overpass with travel lanes, BRT Dedicated Transit Lanes and the Archer Braid Trail	SW 43rd Street to SW 47th Street	0.5	$13.0

Priority	Description	From/To	Length (In Miles)	Estimated Cost In Millions (In 2010 Dollars)
8	NW 83rd Street, new roadway with travel lanes, BRT Dedicated Transit Lanes and the Millhopper Greenway	NW 46th Avenue to NW 39th Avenue (SR 222)	0.4	$2.5
9	NW 83rd Street, BRT Dedicated Transit Lanes	NW 23rd Avenue to NW 39th Avenue	1.0	$7.8
10	Ft. Clarke/NW 83rd Street Corridor, BRT Dedicated Transit Lanes & new multimodal only Interstate 75 overpass	NW 23rd Avenue to Newberry Road (SR 26)	1.0	$14.0
11	NW 46th Avenue, new roadway with travel lanes, BRT Dedicated Transit Lanes, multi-use path and new Interstate 75 overpass	NW 83rd Street to NW 98th Street	1.3	$15.5
TOTAL ALACHUA COUNTY TRANSIT AND ROADWAY SYSTEM				**$83.5**

Priority	Description	From/To	Length (In Miles)	Estimated Cost In Millions (In 2010 Dollars)
City of Gainesville Projects (local funds identified as Cost Feasible by the Year 2020)				
N/A	SE 4th Street- Phase 2 reconstruction	Williston Road to Depot Avenue	0.7	$2.3
N/A	SW 62nd Boulevard-four lanes plus two additional BRT lanes in the middle	Newberry Road to Archer Road	3.2	$111.0
TOTAL CITY OF GAINESVILLE ROADWAY SYSTEM				**$113.3**
GRAND TOTAL COMBINED ROADWAY SYSTEMS				**$265.7**

[a]Multimodal corridors are defined as major transportation facilities which accommodate automobile, truck, bus, bicycle and pedestrian travel and link different modes together, such as bikes on buses, car and walk and/or park and ride. These projects employ policies and design elements that ensure that the safety and convenience of all users of a transportation system are considered in all phases of project planning and development. Typical elements of a multimodal corridor include sidewalks, bicycle lanes (or wide, paved shoulders), shared-use bicycle and pedestrian paths, designated bus lanes, safe and accessible transit stops and frequent and safe crossings for pedestrians, including median islands, accessible pedestrian signals, and curb extensions.

[b]Waldo Road Multiway Boulevard includes the reconstruction of the Waldo Road Corridor to support commercial and residential redevelopment and enhanced pedestrian crossings to the proposed Waldo Road Bus Rapid Transit line.

Note- Estimated costs are shown in Year 2010 dollars, except for the Strategic Intermodal System project that is shown in Year 2009 dollars.

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May 24, 2013

TO: Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area

FROM: Marlie Sanderson, AICP, Director of Transportation Planning

SUBJECT: Available Federal Funds for Tower Road

STAFF RECOMMENDATION

No action required. This material is for information only.

BACKGROUND

A member of the Metropolitan Transportation Planning Organization for the Gainesville Urbanized Area has requested the following agenda item for the June 3, 2013 meeting- *"What federal funds are available for Tower Road from Newberry Road to Archer Road?"*

Tower Road is classified as part of the "federal aid system." Therefore, this project is eligible for federal Surface Transportation Program Funds. In order to receive these funds, projects must be included in the "financially feasible" portion of the adopted long range transportation plan.

The Tower Road Project is currently identified as a needed project in the adopted Year 2035 Roadway Needs Plan (see attached Table 61). However, this project is not included in the adopted Surface Transportation Program Year 2035 Cost Feasible Plan (see attached Table 70). In order for this project to be funded with federal Surface Transportation Program funds, it must be included in Table 70. This could occur in one of the following two ways-

Option 1- Table 70 could be amended to include this project. However, another project(s) would need to be deleted from Table 70 in order for this Plan to remain financially feasible (future project costs must equal projected revenues).

Option 2- The long range transportation plan is scheduled to be updated by October 26, 2015. This project could be addressed when this plan is being updated over the next two years.

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Table 61: Year 2035 Roadway Needs Plan Project Costs

Facility/Location	From/To	Type	Length	Project Cost 2010 $'s (in millions)
Airport Access Road	Waldo Rd to Airport	New 2 lane road	0.5	$ 2.4
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Radio Road Extension	SW 34th St. to Hull Rd Extension	New 2 lane road	1.0	$ 4.5
Springhills Boulevard	NW 83rd St Ext to NW 115th St	New 2 lane road	2.3	$ 20.6
Tower Road	SW 8th Avenue to Archer Road	Reconstruct (2 lane upgrade)	3.2	$ 13.4
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NW 76th Boulevard Extension	NW 76th Blvd to Ft Clarke Blvd.	New 2 lane road	0.6	$ 2.8
NW 83rd Street Extension	NW 39th St to Millhopper Rd	New 2 lane road	1.5	$ 6.7
NW/SW 13th Street	SW 16th Ave to NW 23rd Ave	Multimodal Emphasis	2.6	$ 10.0
SE 4th Avenue	Depot Ave to Williston Rd	Reconstruct (2	0.7	$ 0.8

Facility/Location	From/To	Type	Length	Project Cost 2010 $'s (in millions)
		lane upgrade)		
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I-75 @ SR 26/Newberry Road		SIS Interchange Modification		$ 1.6
I-75 @ SR 331/Williston Road		SIS Interchange Modification		$ 1.6
Total Roadway Needs				$ 372.3

Table 70: Surface Transportation Program Year 2035 Cost Feasible Plan (by Year of Expenditure)

Priority	Description	Project	From / To:	Length (miles)	Project Cost (in 2010 dollars)	2014-2015	2016-2020	2021-2025	2026-2030	2031-2035	Total Cost (YOE dollars)
1	Oaks Mall to Airport Bus Rapid Transit Alternatives Analysis	Alternatives Analysis	Oaks Mall to Airport (via Archer Road and Downtown)	n/a	$0.4	$0.4	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	$0.4
2	Santa Fe to Oaks Mall Bus Rapid Transit Alternatives Analysis	Alternatives Analysis	Sante Fe to Oaks Mall	n/a	$0.6	$0.6	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	$0.6
3	Streetcar Feasibility Study	Feasibility Study	Downtown to Butler Plaza via University of Florida	9.0	$1.0	$1.0	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	$1.0
4	Intermodal Center / Park and Ride Lot	Park and Ride Lot	TBD	n/a	$1.4	$0.1	PD&E PE ROW CST	PD&E PE $ 0.2 ROW CST	PD&E PE ROW $ 0.4 CST	PD&E PE ROW CST $ 1.3	PD&E PE ROW CST	$1.9
5	Transit Maintenance Facility		n/a	n/a	$50.0		PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST	PD&E PE ROW CST $53.0	$32.2

SCHEDULED 2013 MTPO AND COMMITTEE MEETING DATES AND TIMES

PLEASE NOTE: All of the dates and times shown in this table are subject to being changed during the year.

MTPO MEETING MONTH	TAC [At 2:00 p.m.] CAC [At 7:00 p.m.]	B/PAB [At 7:00 p.m.]	MTPO MEETING
FEBRUARY	January 23	January 24	February 4 at 3:00 p.m.
MARCH	February 20	February 21	March 4 at 3:00 p.m.
JUNE	May 22	May 23	June 3 at 5:00 p.m.
AUGUST	July 24	July 25	August 5 at 3:00 p.m.
SEPTEMBER	September 18	September 19	September 30 at 3:00 p.m.
DECEMBER	November 20	November 21	December 2 at 5:00 p.m.

Note, unless otherwise scheduled:

1. Shaded boxes indicate the months that we may be able to cancel MTPO meetings if agenda items do not require a meeting and
   corresponding Advisory Committee meeting may also be cancelled;
2. TAC meetings are conducted at the Gainesville Regional Utilities (GRU) Administration general purpose meeting room;
3. CAC meetings are conducted in the Grace Knight conference room of the County Administration Building; and
4. MTPO meetings are conducted at the Jack Durrance Auditorium of the County Administration Building unless noted.

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Metropolitan Transportation Planning Organization
for the Gainesville Urbanized Area

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